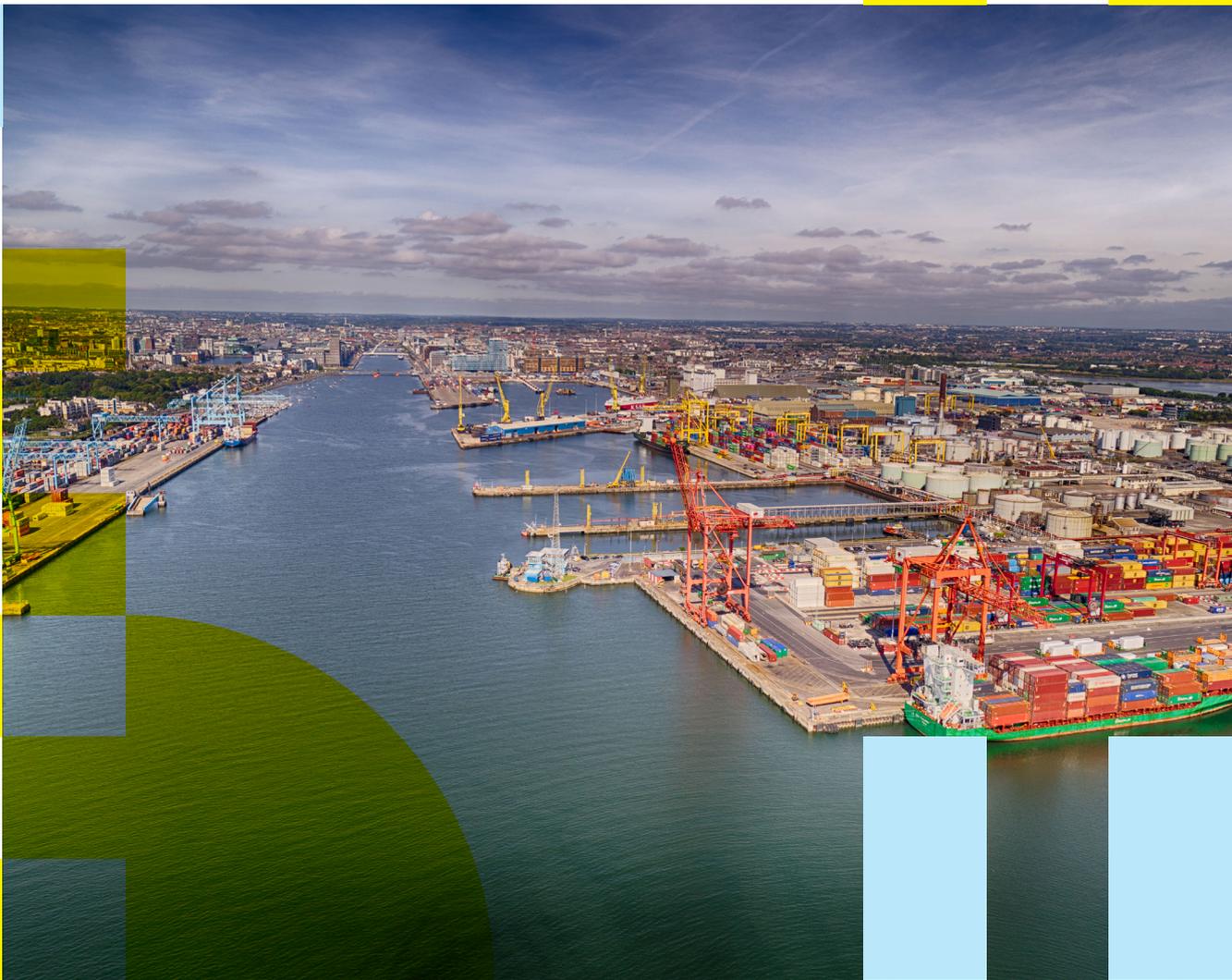


Environmental Impact Assessment Report

Appendix 16.1

Volume 3 Part 8



Appendix 16-1

Catalogue of Cultural Heritage Assets

Compiled by Niall Brady and Dominick Gallagher

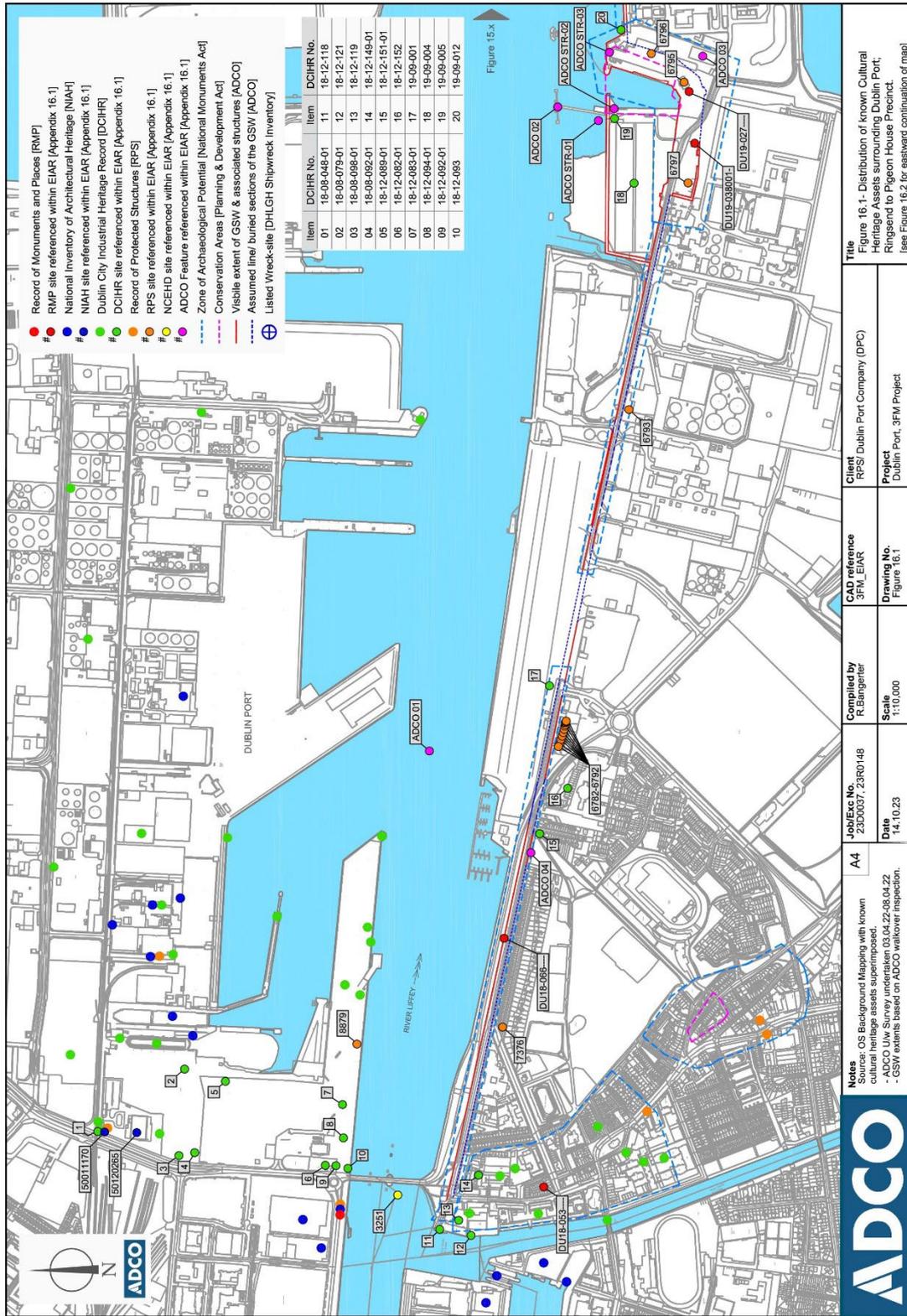
The catalogue is based on the known cultural heritage sites recorded within the development area and adjoining it. The entries are drawn from the National Monuments Service's Register of Monuments and Places (RMP), the National Inventory of Architectural Heritage (NIAH), Dublin City Council's Record of Protected Structures (RPS), the Dublin City Industrial Heritage Record (DCIHR), the National Civil Engineering Database (NCEHD) and Dublin Docklands Industrial Archaeology Survey (DDIAS).

Supplemental information is provided where appropriate from the Dublin Port Archives and from field walkover inspection carried out to inform the 3FM project. Additional information based on record taken during archaeological monitoring of Site Investigations works for the 3FM project is described in Appendix 16-4, and on record taken during the underwater archaeological impact assessment for the 3FM project is described in Appendix 16-5.

Historic images are sourced from Dublin Port Archives unless otherwise indicated. Present-day photographs are sourced from ADCO's field inspections for DPC unless otherwise indicated.

The catalogue provides the official site registration reference; site name; site type; positioning data based on ITM Easting and Northing; Site Description; Proximity to the 3FM development area; Likely impacts and Recommended mitigation arising from the 3FM project. Where sites are included that have no official site reference number, they are given a project specific number; namely, ADCO #.

The catalogue entries are summarised in Chapter 16 Table 16-3, and are presented below following same order. The locations of each site are presented in Figures 16-1 and 16-2, which are included in Chapter 16 and are repeated in this Appendix for ease of reference.





North Side of the Channel

Reference	Site Name	Site Type	ITM Easting	ITM Northing
DCIHR 18-08-048-01	Level Crossing	Level Crossing	718129	734913
Description	Railway continues to crossroads though no trace of level crossing survives. Traffic lights now regulate crossing of road			
Image/s	None			
Proximity to development	Adjacent to development area			
Likely Impacts	None			
Recommended Mitigation	None			

Reference	Site Name	Site Type	ITM Easting	ITM Northing
NIAH 50011170	Substation	Substation	718133	734901
Description	<p>The redbrick building that stands at the junction of East Wall Road and Alexandra Road in Dublin Port is a former electricity substation that was built in 1922, the year of Irish Independence. It has been rehabilitated in 2023 by Dublin Port Company as a small event space known as the Substation. The rehabilitation works were monitored archaeologically by ADCO under licence 21E0737 and in fulfillment of Dublin City Council's grant of planning, reference 2681/20.</p> <p>The Substation was built over the eighteenth-century sea wall that gives its name to East Wall Road and served as a dyke within which the city's growth was able to develop the North Lotts and what became the East Wall. The sea wall is a significant structure whose full width probably extends across much of East Wall Road. The element that has been discovered under the redbrick building is the eastern edge of the sea wall.</p>			
Image/s	 <p>View looking east from East Wall Road towards the Substation</p>			



Excavated length of the former sea wall that lies under the Substation

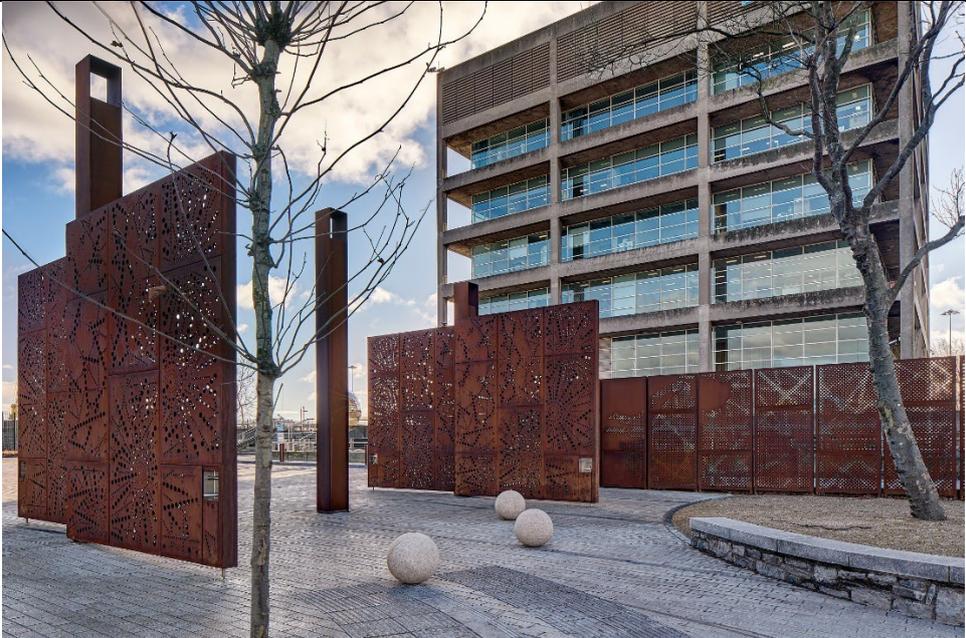


ADCO laser-scan survey showing the exposed extent of the sea wall underneath the Substation

Proximity to development	Adjacent to development area
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Likely Impacts	None
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Recommended Mitigation	None
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Reference	Site Name	Site Type	ITM Easting	ITM Northing
NIAH 50120265	Port Centre	Building	718138	734827
Description	Designed by Scott Tallon Walker and completed in 1981, Port Centre is a detached six-storey office building over basement, supported on single-height podium and is considered to be a prominent and striking office building that creates an architectural focal point at the entrance to Dublin Port, marking the transition between the urban fabric of the city to the west and the industrial landscape to the east. Construction of Port Centre was within the former shipbuilding yard.			
Image/s	 <p>View looking south from East Wall Road to Port Centre and including elements of the rehabilitated entrance plaza. Photograph by Enda Kavanagh for DPC</p>			
Proximity to development	Adjacent to development area			
Likely Impacts	None			
Recommended Mitigation	None			

Reference	Site Name	Site Type	ITM Easting	ITM Northing
DCIHR 18-08-079_01	Port and Docks Depot	Building, site of	718254	734736
Description	Building recorded on OS 3rd Edition 6-inch map. No trace upstanding.			
Image/s	None			
Proximity to development	Adjacent to development area			
Likely Impacts	None			
Recommended Mitigation	None			

Reference	Site Name	Site Type	ITM Easting	ITM Northing
DCIHR 18-08-098_01	Light House	Light House, site of	718073	734746
Description	Light house recorded on OS 1847 map. No trace upstanding			
Image/s	None			

Proximity to development	Adjacent to development area
Likely Impacts	None
Recommended Mitigation	None

Reference	Site Name	Site Type	ITM Easting	ITM Northing
DCIHR 18-08-092_01	Gasometer	Building, site of	718075	734721
Description	Gasometer recorded on OS 1910 map. No trace upstanding			
Image/s	None			
Proximity to development	Adjacent to development area			
Likely Impacts	None			
Recommended Mitigation	None			

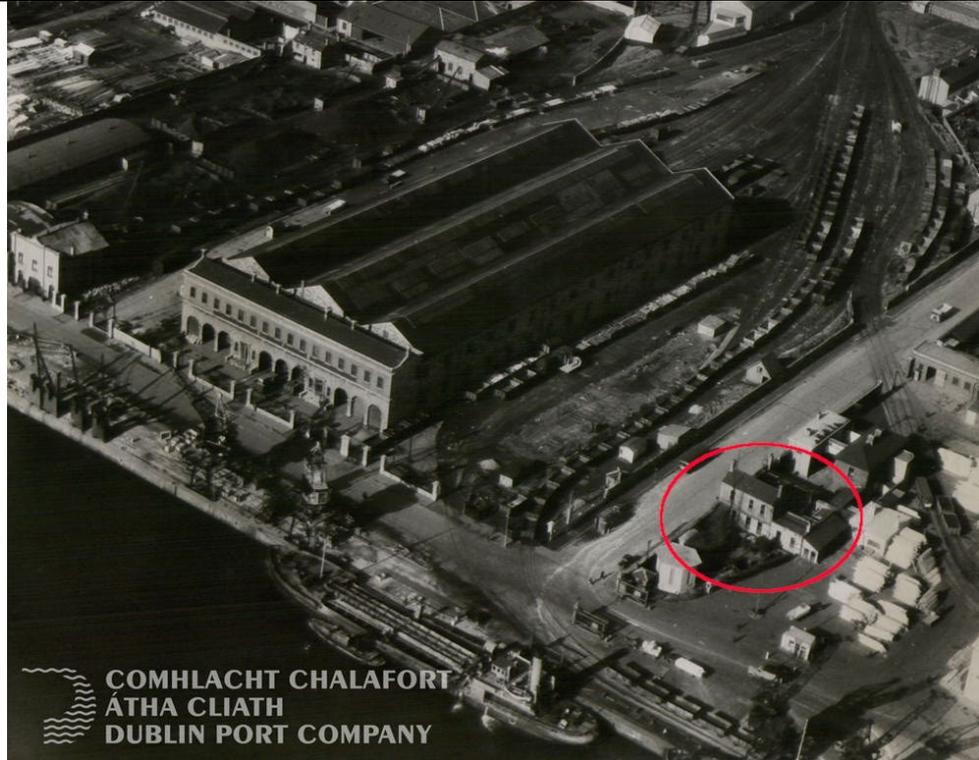
Reference	Site Name	Site Type	ITM Easting	ITM Northing
DCIHR 18-12-089_01	Patent Slip No 2	Slip	718225	734627
Description	Patent Slip No 2 was recorded on historic OS maps and was exposed and archaeologically recorded by ADCO under the permitted Alexandra Basin Redevelopment Project, archaeological licence 16E0212.			

Image/s



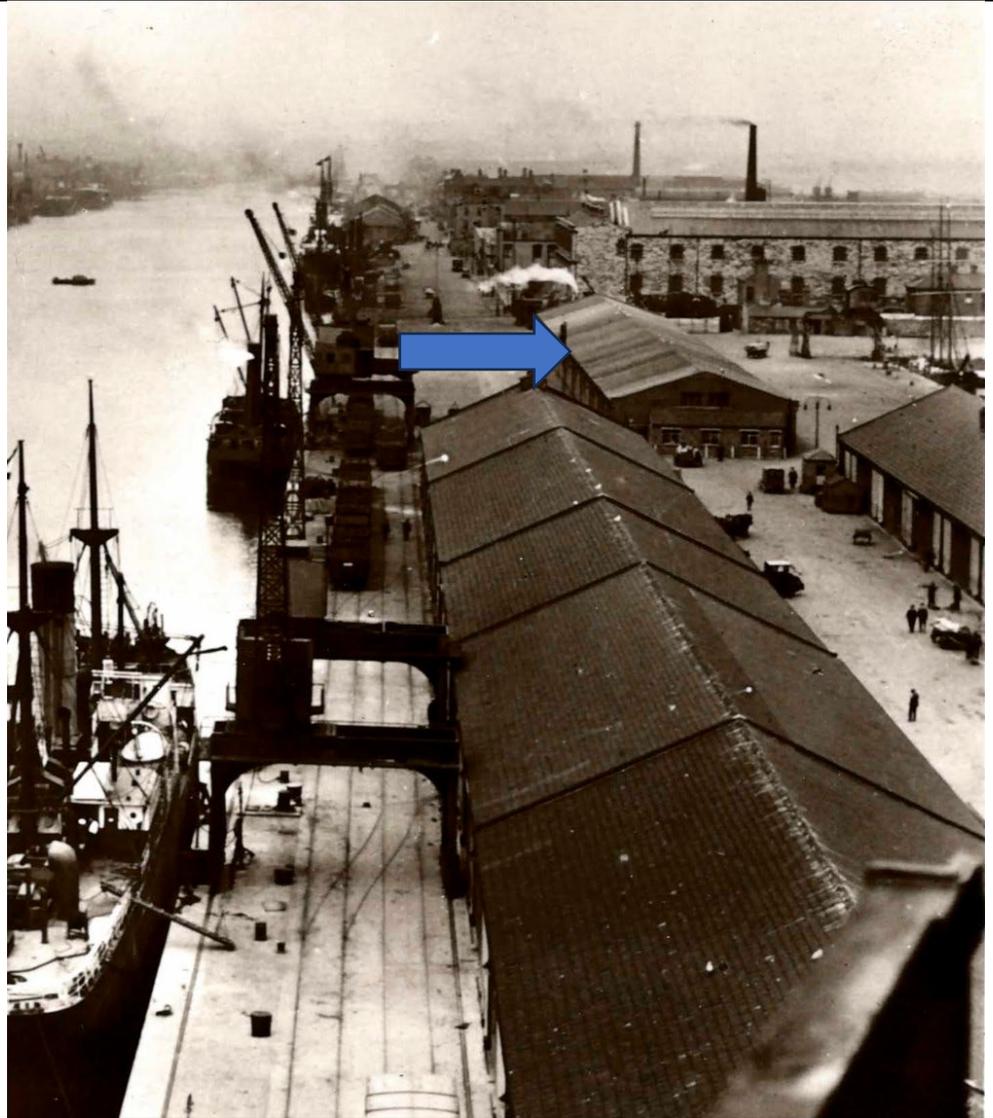
Proximity to development	Adjacent to development area
Likely Impacts	None
Recommended Mitigation	None

Reference	Site Name	Site Type	ITM Easting	ITM Northing
DCIHR 18-12-082-01	Harbour Master's Office	Building	718060	734451
Description	Constructed c. 1880, the building was located along East Wall Road at Crossberth Quay, close to the entrance of the North Wall Quay Extension. Possibly replaced earlier office buildings at this location. In 1890, the roof of the Harbour Master's Office was taken down, and the building was raised and enlarged to give greatly improved accommodation. An enclosed yard and new sanitary appliances were also provided. The building was demolished in c. 1992 and area now functions as a car park.			

<p>Image/s</p>	
<p>Proximity to development</p>	<p>Outside development area</p>
<p>Likely Impacts</p>	<p>None</p>
<p>Recommended Mitigation</p>	<p>None</p>

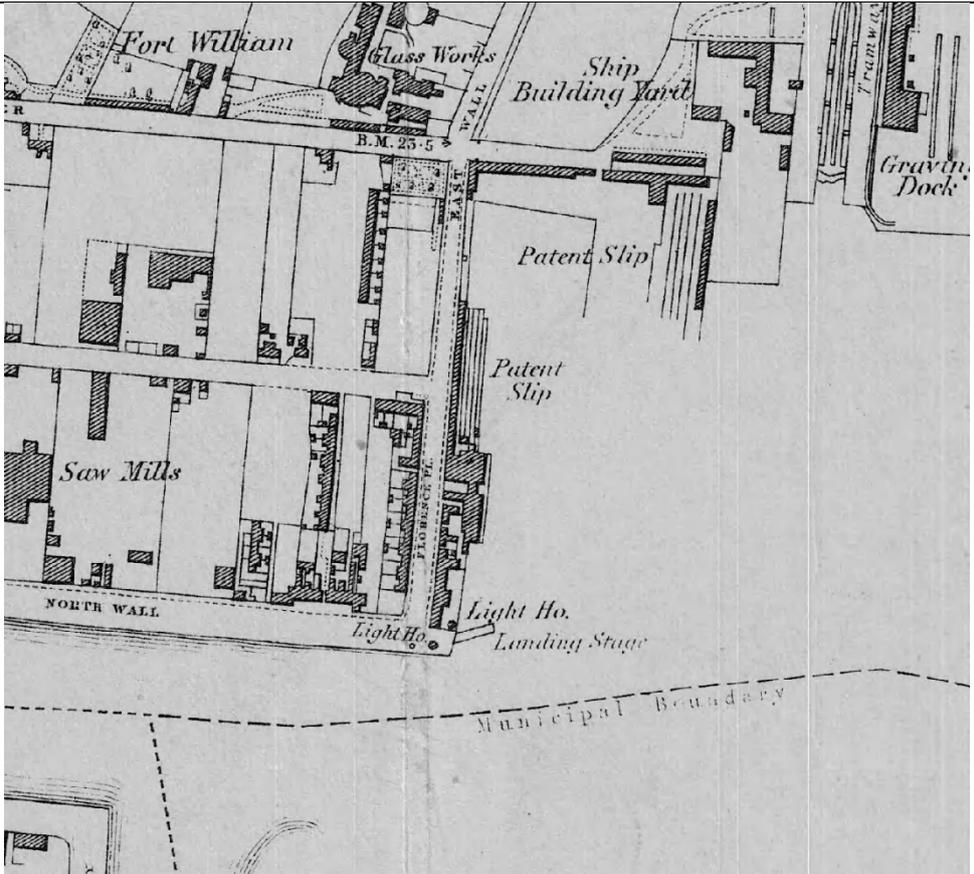
Reference	Site Name	Site Type	ITM Easting	ITM Northing
DCIHR 18-12-083-01	Goods Shed	Goods Shed	718181	734407
Description	One of the sheds of the North Wall Quay Extension (in foreground of picture). Sheds were built on North Wall Quay Extension shortly after its construction and were demolished in c. 1990s.			

Image/s



Proximity to development	Within development area
Likely Impacts	SPAR will cross over site. See Figure 5.5 in Chapter 5 of EIAR.
Recommended Mitigation	Archaeological monitoring and resolution

Reference	Site Name	Site Type	ITM Easting	ITM Northing
DCIHR 18-12-094-01	Landing Stage	Landing Stage	718109	734403
Description	Historic record of feature that was demolished presumably when NWQE was constructed.			

<p>Image/s</p>	 <p>Extract of OSI 1864</p>
<p>Proximity to development</p>	<p>Adjacent to the development area</p>
<p>Likely Impacts</p>	<p>None</p>
<p>Recommended Mitigation</p>	<p>None</p>

Reference	Site Name	Site Type	ITM Easting	ITM Northing
DCIHR 18-12-092-01	Lighthouse		718062	734414
<p>Description</p>	<p>DCIHR Description and appraisal: Function at time of survey: Quay. No Remains identified.</p> <p>From available cartographic sources, no lighthouse is illustrated close to this location until the publication of the 1832 County of City, Boundary Commission and the OS 6-inch map. The Lighthouse Directory dated a lighthouse at the east end of the North Wall to 1820. However, it does seem plausible that some form of light was positioned here before 1820. There is a Revenue Watch House depicted in several paintings (1745 and 1793) at the junction of the North Wall Quay and East Wall and in neither is there a sign of a lighthouse. Taylor's map of 1816 shows this watch tower to have been switched to the opposite side of the river to Ringsend point, however no lighthouse is depicted to have taken its place.</p> <p>In 1786 a law established the Corporation for Preserving and Improving the Port of Dublin for lighthouses in the immediate area of the Port, and later in 1810 the Corporation assumed responsibility for the erection and maintenance of all lighthouses along the coast of Ireland. This responsibility was transferred to the Commissioner of Irish Lights in 1867. It is probable that the lighthouse at the east end of the North Wall was erected by the Corporation and dismantled by the Commissioner of Irish Lights.</p> <p>In a survey of sailing direction along the Coasts of Ireland dated 1842, a reference is made to 'A circular Iron lighthouse of a gray stone colour on the south-east angle of the North</p>			

	<p>Wall, directly opposite Ringsend shows a fixed white light at an elevation of 29 feet, visible in clear weather 10 miles'.</p> <p>It is possible that lighthouse was moved at some time. Maps have slightly differing locations for a lighthouse at this location. It is possible that this lighthouse is an earlier or later iteration of lighthouse DCIHR 18-12-093.</p>
Image/s	None
Proximity to development	Outside development area
Likely Impacts	None
Recommended Mitigation	None

Reference	Site Name	Site Type	ITM Easting	ITM Northing
DCIHR 18-12-093	North Wall Quay Light	Lighthouse	718042	734390
Description	<p>Lighthouse at the entrance to the Liffey at the junction of the North Wall Quay and East Wall Road. Built c. 1820. Showed a steady light at a height of 33 feet and could be seen from a distance of 8 miles. Lighthouse contained a 5th order fixed lenticular light.</p> <p>Lighthouse was still present in 1864 during the launch of Knights Commander and was probably dismantled during the construction of North Wall Quay Extension. First addition Ordnance Survey 6-inch shows lighthouse. Not visible of Taylor's map 1816. Entrance gates to NWQE are now at this location.</p>			
Image/s	 <p>Historic image showing the Light house and Dublin Bay Gabbards. Image courtesy of Cormac Lowth</p>			

	
	<p>A view of the North Wall, City of Dublin. <i>Dublin Penny Journal</i> 1834</p>
<p>Proximity to development</p>	<p>Outside development area</p>
<p>Likely Impacts</p>	<p>None</p>
<p>Recommended Mitigation</p>	<p>None</p>

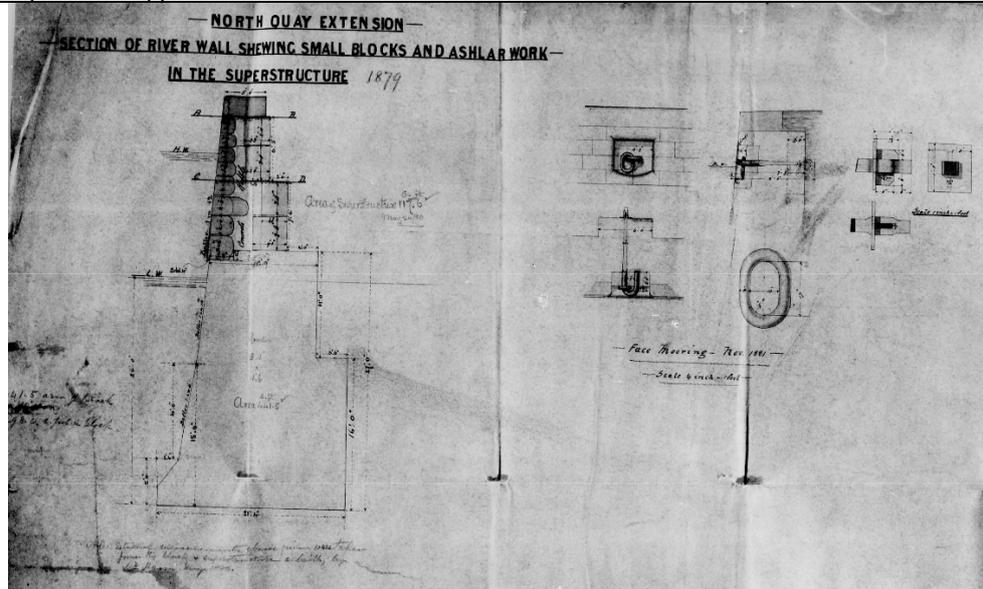
Reference	Site Name	Site Type	ITM Easting	ITM Northing
<p>RPS 8879; DCIHR 18-12-084-01</p>	<p>North Wall Quay Extension</p>	<p>Quay</p>	<p>718113</p>	<p>734378</p>
<p>Description</p>	<p>North Wall Quay Extension (NWQE) was constructed to enclose the southern part of the new deepwater basin that was built off the seaward side of East Wall Road. Construction began in 1869 and adopted the innovative design of Port Engineer Bindon Blood Stoney, who used Portland cement along with stone to make 350-tonne foundation blocks strengthened with iron girders. The blocks were cast on a block wharf located on the north shore of the deepwater basin and were floated across and then lowered into position, using Stoney's purpose-built vessel, his Shears Float. He also designed the Diving Bell so that workers could prepare the seabed in advance of lowering the blocks into position. Each block measured 21'4" wide at the base and varied in height between 26' and 29'. The blocks had a stepped profile in cross-section that was recessed by 3'6" at the rear and was of varied height, between 9'6" and 13'. The blocks form a continuous façade on both sides of the NWQE, with dredged sands filling the interior.¹ Granite ashlar was added to the blocks above the Low Water Mark, in keeping with the style of the city quays upstream. Stoney's construction continued until 1885 at which point 700m of the quay was built. Completion of the NWQE had to wait until the early twentieth century when, under the direction of Joseph Mallagh, the eastern terminus was completed in time for the Eucharistic Congress in 1932, employing a different method of construction that employed pre-cast caissons.</p> <p>Dublin Port Archives retains a series of engineering drawings that provide measured detail concerning the quay design and its furnishings as well as information on repairs.</p> <p>Archaeological inspection completed for the Alexandra Basin Redevelopment Project EIS confirmed the essential detail of the quay wall above and below sea level. The exposed facades of the quay survive largely untouched. The original mooring rings set into the quay wall are retained and elegant cut-stone steps are still in use. The granite blocks of the quay wall are however in various state of deterioration. The coping stones on the quay deck</p>			

¹ Ronald Cox, *Bindon Blood Stoney. Biography of a Port Engineer* (Dublin, 1998).

survive in places along the quay edge. The interior surface of the deck is masked by cobble-lock brick that conceals the earlier features on the quay's working surface but lengths of rail line are retained that speak to earlier uses. Underwater inspection (completed under licence 13D0019) also revealed the concrete foundation blocks, the toes of which can be partly buried in silt or exposed.

Further underwater archaeological inspection was carried out for the 3FM project, including detailed measured survey of an 80m-long stretch of the quay and supporting description is reported in Appendix 15-5.

Image/s



Detail from Dublin Port Archive engineering drawing reference 7929 dated 1879, showing section of quay wall highlighting a foundation block and the ashlar coursing placed above, and detail of a mooring ring.



Photograph shows the top of two mass concrete foundation blocks and the join between both at the base of the image, with the granite ashlar coursing constructed above and a recessed mooring ring

Proximity to development

Within development area

Likely Impacts

SPAR Bridge will cross over the NWQE. See Figure 1.2 in EIAR Impacts arising:

	<ul style="list-style-type: none"> • Capstones and upper courses of granite ashlar to be removed across bridge footprint • Excavation of NWQE surface to working depth of xxm across bridge footprint • Impact with one recessed mooring ring and associated stonework anticipated
Recommended Mitigation	<ul style="list-style-type: none"> • SPAR Bridge designed to minimise impact on NWQE • Supplemental detailed archaeological topographic survey prior to construction • Avoid impact with recessed mooring ring and associated stonework • Archaeological monitoring and resolution • Retention of ashlar granite for sympathetic re-use • Rebuild recessed mooring ring and associated stonework if impacts are unavoidable

Channel

Reference	Site Name	Site Type	ITM Easting	ITM Northing
ADCO 01	Liffey	Riverbed	7181093	734292
Description	The Liffey channel within the development area has a silt bed over gravel. It has been impacted on by dredging, by the development of the quaysides on the south Port and by development of the Tom Clarke Bridge in the 1970s with the associated reclamation of the southern foreshore. The channel was inspected archaeologically for the ABR project and was recently inspected archaeologically for the 3FM project and is reported on in Appendix 15-5.			
Image/s	 <p>Photograph from 1970s showing construction of the Tom Clarke Bridge and reclamation works along the south bank in advance of the toll road. Source: Roughan O'Donovan.</p>			
Proximity to development	Within development area. See Figure 1.2 in EIAR			
Likely Impacts	<ul style="list-style-type: none"> • SPAR Bridge will cross over site, supported by six in-water piers • SPAR will continue along the south shore as an elevated road supported on piles • Temporary relocation of Poolbeg Yacht and Boat Club Marina moorings will be secured by a series of weighted blocks off the NWQE, with no impacts on to the buried riverbed deposits and no impacts on to the NWQE • Capital dredging works will take place to facilitate new berths associated with the Maritime village • The Turning Circle proposed in front of Pigeon House Harbour will require capital dredging, and construction of a revetment for a portion close to the harbour • The Sludge Jetty and adjacent timber pier will be removed • Area N will be constructed on piles • Some element of capital dredging is anticipated associated with Area N 			
Recommended Mitigation	Archaeological monitoring and resolution			

Reference	Site Name	Site Type	ITM Easting	ITM Northing
NCEHD 3251	Liffey Services Tunnel	Tunnel	717995	734285
Description	Tunnel constructed across the Liffey channel between North Wall Quay and York Road, to serve as a services tunnel bringing water and electricity from the south side of the city. The tunnel was dug 100 feet below ground on the northern site, and ran from close to the			

	Harbour Master's Office () under the river to York Road.Thorncastle Street. The tunnel measured 830 feet long (253m) and 11 feet in diameter (3.4m), and was constructed in brick. It was completed in 1929. Archaeological monitoring in 2007 in the vicinity of the tunnel encountered stone walls, possibly former quay walls.
Image/s	none
Proximity to development	Outside the development area, crossing the river channel upstream of the East Link Bridge.
Likely Impacts	None
Recommended Mitigation	None

South side of the Channel: Great South Wall and Pigeon House Fort and Precinct

The Great South Wall (GSW) today is an amalgamation of three principal elements: the Ballast Office Wall; Pigeon House Harbour and Fort, and the GSW east of the fort to Poolbeg Lighthouse. Though recorded in the official registers as separate features, they are considered here as a contiguous whole. A series of other sites and features that are built on to the GSW are described in this section as well. To ease orientation, the GSW and the attendant features along it will be presented in three blocks:

1. GSW to Pigeon House Harbour
2. Pigeon House Harbour, Fort and Precinct
3. GSW east to Poolbeg Lighthouse

1. GSW to Pigeon House Harbour

Reference	Site Name	Site Type	ITM Easting	ITM Northing
DU018-066; RPS 6797	Sea Wall/Ballast Office Wall	Sea Wall	Various	Various
Description	<p>The Ballast Office Wall is the second main stage in the construction of the Great South Wall. In 1748, the Ballast Board announced plans to construct a double wall in stone that would extend 2,100m from the west end of The Piles to Ringsend Point. It was completed in 1759 and is recorded on John Rocque's 'Survey of the City, Harbour, Bay and Environs of Dublin' map of 1757 as 'The South Wall'. The wall was built with two parallel rubble walls and the intervening space (varying between 37 and 48 feet in width) filled with sand. The wall was furnished with parapets above the road surface, which served as a barrier between the road and the sea for those who travelled along the wall to and from Pigeon House harbour.</p> <p>A cross-section of the wall is included in Isaac Mann's essay on the Dublin Bar (1881), and this has served as a principal source, used subsequently by De Courcy (1996).</p> <p>Today this length of wall lies under York Road and Pigeon House Road. For ease of orientation, the description and accompanying images is divided into three sections:</p> <ul style="list-style-type: none"> • York Road-Pigeon House Road to Ringsend Park • Poolbeg Yacht and Sail Club to MTL Yard • Murphy's Yard to Pigeon House Harbour <p><u>York Road-Pigeon House Road to Ringsend Park:</u> Rocque's map shows the wall as being open-ended on its most upstream point, where the wall begins to curve south in the direction of what is today Thorncastle Street. Certainly the present-day waterfront at the Liffey/Dodder confluence dates from later decades and is associated with the nineteenth-century presence of industrial sites (see DCIHR 18-12-118 and 18-12-121 below).</p>			

An aerial photograph taken in the early 1930s shows a slight outward kink into the quay wall upstream of Thorncastle Street, supporting the view that waterfront at the Liffey/Dodder confluence is a later addition. The photograph also shows the shallow nature of the channel against the base of the GSW and a series of floating moorings in the river much as moorings are present today at this location.

The north-facing parapet of the sea wall survives today along York Road, where it can be seen as far west of St Patrick's Rowing Club, and as far east as the adjoining section of Pigeon House Road that runs alongside Ringsend Park. The parapet is the boundary wall between York Road and the R131 toll road. While there are several breaches in the wall along this stretch, it also retains a recessed vertical quay ladder.

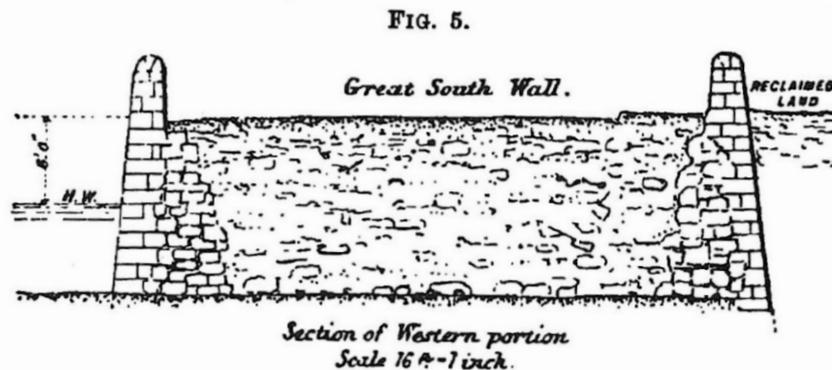
In contrast, the south-facing parapet is largely absorbed into the current building fabric, where the houses that front on to York Road are constructed directly on top of the parapet. However, a portion survives at the junction of York Road and Pembroke Cottages. Further elements survive as the boundary wall of Ringsend Park.

A Slipway is recorded off the south-facing side of the wall on Rocque's map of 1757, labelled St. George's Slip (ADCO 04 in this catalogue). A second slipway recorded by Rocque, Mackarel's Slip is recorded as DCIHR 19-09-001.

Image/s



Detail from Rocque 1757, showing the South Wall as constructed prior to its completion in 1759. Note the two slipways attendant to the wall, referenced ADCO 04 and DCIHR



Cross-section of the Ballast Office Wall recorded in Mann, 1881.



Detail from aerial photograph taken in early 1930s looking upriver, with the GSW/Pigeon House Road on the left, and the NWQE and Alexandra Basin on the right side



Two views of northern parapet looking south, showing quay wall ladder retained



View showing breach in north-facing parapet, and view looking east along north-facing parapet from York Road



Views of surviving south parapet wall at junction of York Road and Pembroke Cottages



View looking east along York Road where the south-facing parapet does not survive above ground but is replaced with the curtilage fencing of Ringsend College



View looking east along Pigeon House Road where the south-facing parapet survives as the standing boundary wall with Ringsend Park

	
<p>Description</p>	<p>View looking east along Pigeon House Road where the south-facing parapet survives as the front garden boundary wall to residential properties at junction with Cambridge Avenue Poolbeg Yacht and Sail Club to MTL Yard: The R131 has breached the GSW and cuts across the sea wall at an oblique angle.</p> <p>Pigeon House Road continues to the east of the R131 and the front garden walls of the former coastguard cottages (RPS 6782–RPS6792, see below) follow the line of the southern parapet. In contrast, the northern parapet is not upstanding but is most probably underneath a low concrete retaining wall that serves as the boundary with the northern reach of the MTL yard.</p> <p>Within the MTL Yard, a length of the GSW is apparent where a green verge extends for c. 122m. The north-facing wall that retains the grassed area is the southern parapet of the GSW. The northern limit of the GSW does not survive upstanding across the Yard.</p>
<p>Images</p>	 <p>View looking east where R131 (to right) breaches the GSW, and where Pigeon House Road (left) continues</p> <p>View looking west along Pigeon House Road, The parapets of the GSW are not upstanding at this point, and have been built over</p>



View looking west along Pigeon House Road, showing the southern parapet of the GSW as it survives serving as the boundary wall to the Ringsend Sea Scouts den



View looking east along upstanding southern parapet of the GSW that retains the north side of the grass verge in the MTL Yard



The south parapet of the GSW within the MTL Yard has been breached in several places, in this instance to insert step access



Detail showing fabric of the south parapet of the GSW in the MTL Yard where the wall core is exposed

Description

Murphy's Yard to Pigeon House Harbour:

At the junction of Whitebank Road and Pigeon House Road, the parapets of the GSW re-emerge. The southern parapet is preserved and serves as the northern boundary to Murphy's Yard. A 32 m-long stretch has been surveyed in detail.² The long façades of the wall are slightly battered, or sloped, and rise to a height of 800 mm above the current pavement. At this point, the top of the wall has a rounded cap that is 250 mm high and is made from stone cobbles. The main stone of the wall appears to be a schist, and the

² Niall Brady, 'Archaeological assessment, boundary wall, Pigeon House Road. CP1501-026, Compound Wall'. Report of ADCO for Dublin Port Company, 2019.

average stone size measures 550 mm long and 850mm high. A dirty white coloured mortar with small stone inclusions bonds the stone. The wall has no formal coursing but three layers of stonework are evident, with smaller stone chips used to fill in the gaps. The wall is not plumb and, as one proceeds eastwards, various patches are evident. There is no brickwork evident but modern concrete is used to cover over the stonework that creates the rounded cap. The wall top is at 5.31 m above Ordnance Datum. The parapet is breached and brick gateposts are inserted across a 3.4 m-wide entrance into Murphy's Yard. A second break further east provides entrance to the ESB substation. From this point onwards, the eastward extent of the southern parapet is not upstanding and has been replaced with modern blockwork.

The northern parapet survives opposite Murphy's Yard but is more ruinous. The footings for the stone wall stand to a height of 330 mm below a dilapidated cement breeze-block-built low wall. The original width of the stone wall above the current pavement appears to be 580 mm. The stone is the same type as seen on the southern parapet. The northern wall is breached at an entrance to the MTL Yard. East of the MTL Yard entrance, the northern parapet wall is once again evident and it continues as a low stone wall along the remaining length of the MTL Yard, at which point it is breached by the roadway leading to Hammond Lane. The low wall height suggests that much of this stretch is rebuilt as a dwarf wall.

An historical photograph taken in 1922 and showing the departure of British cavalry from Pigeon House Fort indicates the height of the parapets above the original road surface. The image may have been taken in this general location. The detail confirms the battered profile recorded at Murphy's Yard and also suggests the parapets stood perhaps 1.5 m above the original road surface. Given that the wall at Murphy's Yard is 800 mm above the current road surface, it suggests that the modern road fill may be in the order of c. 700 mm deep.

Between Hammond Lane and Pigeon House Harbour, there is no upstanding element of the southern parapet, where the south side of the road is taken up mostly by the remains of the Fever Hospital (RPS 6793) whose boundary wall is red brick. Low stone footing is evident to the west of the Hospital but it is not clear what period the footing belongs to. Following the breach created by South Bank Road, the southern boundary of Pigeon House Road is a simple metal fence that serves the Waste to Energy facility.

In contrast, the full depth of the sea wall survives and is visible on the north side of Pigeon House Road, as the sea wall drops over 5 m to the seabed that is now used as the cooling water area for the Waste to Energy facility. Archaeological monitoring of services redirection for the South Quay Access project in 2009 along the north side of Pigeon House Road recorded the corner of some of the inner coping stones of the GSW below the current footpath surface of Pigeon House Road.³ While none of the coping stones were fully exposed, the archaeologist believed the sea wall to be a later addition, measuring 750 mm wide with a semi-circular mortared top, built flush to the outer face of the GSW. The report does not indicate how much later the parapet was added and does not include clear section drawings to explain the observation and conclusion. It remains possible that the parapet as a later addition may be a construction detail that was included to complete the GSW. Alternatively, it was added some time later. Either way, it is today an integral part of the GSW and provided a safe barrier for those travelling along the wall from Ringsend to the harbour and fort.

Archaeological monitoring of site investigations trenches associated with the Dublin Waste to Energy project in 2014 on Pigeon House Road recorded a metal surface and walling, and were interpreted as the remains of the causeway leading to the Fort.⁴ What is of note is that the work encountered a metal surface rather than laid granite slabs, which supports the record of Isaac Mann in 1881.

³ David O'Connor, 'Archaeological monitoring of the proposed South Quay Bridge and Access Road at Pigeon House Road, Dublin Port. 08E0961', report of CRDS for Dublin Port Company, 2010.

⁴ Fintan Walsh and David McIlreavy, 'Dublin Waste to Energy Project, Poolbeg, Dublin 4. 13E0066'.

Images



View looking east along upstanding southern parapet at Murphy's Yard



View looking east at section across parapet wall at Murphy's Yard



View looking west along ruined wall base over northern parapet opposite Murphy's Yard



Historical photograph showing British cavalry on Pigeon House Road, departing the Fort in 1922. At this stage, the road would have been in existence for over a century-and-a-half and no doubt had seen certain adaptation and repair works. It shows a footpath on the south side of the road. Note the height of the parapet walls above what is likely to be original road surface. Source image: Dublin Port Archives



View looking east along Pigeon House Road with Fever Hospital (RPS 6793) in distance, and northern parapet on left side



View looking south at sea wall as it drops over 5 m to active seabed opposite the Fever Hospital

Proximity to development
Likely Impacts

Within development area

York Road-Pigeon House Road to Ringsend Park:

- Pedestrian crossing of R131 from Ringsend Park to be established as part of the creation of a public plaza that celebrates the GSW and allows safe access to the Maritime Village from the south. See Figure 1.2 in EIAR.
- Breach across line of GSW for the new pedestrian crossing at a location where there is currently no upstanding remains.
- Resurfacing of current road surface for new planting and new surface for public plaza.

Poolbeg Yacht and Sail Club to MTL Yard:

- Breach across line of GSW to provide vehicular access to Maritime Village. The impact will be on the north side and central part of the GSW. The southern parapet area will not be impacted. See Figure 1.2 in EIAR.

	<ul style="list-style-type: none"> Breach across line of GSW within MTL Yard where the grass verge and southern parapet will be reduced in height by 400-500 mm over a horizontal area of xx m to facilitate vehicular movement. See Figure 1.2 in EIAR. <p><u>Murphy's Yard to Pigeon House Harbour:</u></p> <ul style="list-style-type: none"> Breach across line of GSW at Murphy's Yard to provide vehicular access. The impact will be on the south side and central part of the GSW. The southern parapet area will be removed over a length of c. 32 m. See Figure 1.2 in EIAR. Widening of junction with access road into Ecocem and Berth 47. Widening of junction with Shellybanks Road. See Figure 1.2 in EIAR.
Recommended Mitigation	<p><u>York Road-Pigeon House Road to Ringsend Park:</u></p> <ul style="list-style-type: none"> Detailed supplementary archaeological survey prior to construction Design of access point to incorporate surfacing to allow for legibility and interpretation of the line of the GSW Interpretive/ wayfinding proposal to be provided in Area K View corridor to be incorporated through Area K that includes changes to surfacing and visual indicators to mark line of GSW Archaeological monitoring and resolution. <p><u>Poolbeg Yacht and Sail Club to MTL Yard:</u></p> <ul style="list-style-type: none"> Detailed supplementary archaeological survey prior to construction to record upstanding parapet elements Design of access point to incorporate surfacing to allow for legibility and interpretation of the line of the GSW Interpretive/ wayfinding proposal to be provided in Area K View corridor to be incorporated through Area K that includes changes to surfacing and visual indicators to mark line of GSW Archaeological monitoring and resolution. <p><u>Murphy's Yard to Pigeon House Harbour:</u></p> <ul style="list-style-type: none"> Supplementary detailed archaeological topographic survey prior to demolition of parapet wall at Murphy's Yard, to record the upstanding south parapet. Re-use of demolition stone to rebuild/rehabilitate the north parapet wall opposite Murphy's Yard. Rehabilitate/make good the parapet wall along Pigeon House Road where the wall is in DPC ownership.

GSW to Pigeon House Harbour, attendant features

Reference	Site Name	Site Type	ITM Easting	ITM Northing
DCIHR 18-12-118	Boat slip	Boat Slip	717920	734198
Description	Exterior Description: Concrete boat slip c.1920-30, with iron posts and railings and concrete mooring blocks. Cast iron lamppost to northwest corner. Limestone masonry quay wall borders to east. This early twentieth-century boat slip retains original railings, etc. and is an important surviving example of maritime construction within an area that has been largely redeveloped. Slip built on the site of an earlier wooden pier as seen on the OS 3rd edition 6-inch map of 1912.			

Image/s



The slip as seen today.



Slipway in 1950s at left. Hammond Lane Metal works now at Hibernian Bottle site and Landing Stage (DCIHR 18-12-121) to right.

Proximity to development

Outside development area

Likely Impacts

None

Recommended Mitigation

None

Reference	Site Name	Site Type	ITM Easting	ITM Northing
DCIHR 18-12-121	Landing Stage	Landing Stage	717909	734127
Description	Remains of a landing stage built c.1900, possibly to serve coal yard located to the east. Later served Hammond Metals from 1950s. Timber planks forming deck supported on metal framework, tied into stone revetment wall to eastern bank of river. Though no longer used and having fallen into disrepair, this landing stage provides an important reminder of the industrial heritage of the River Dodder. Located at the mouth of the river as it enters the			

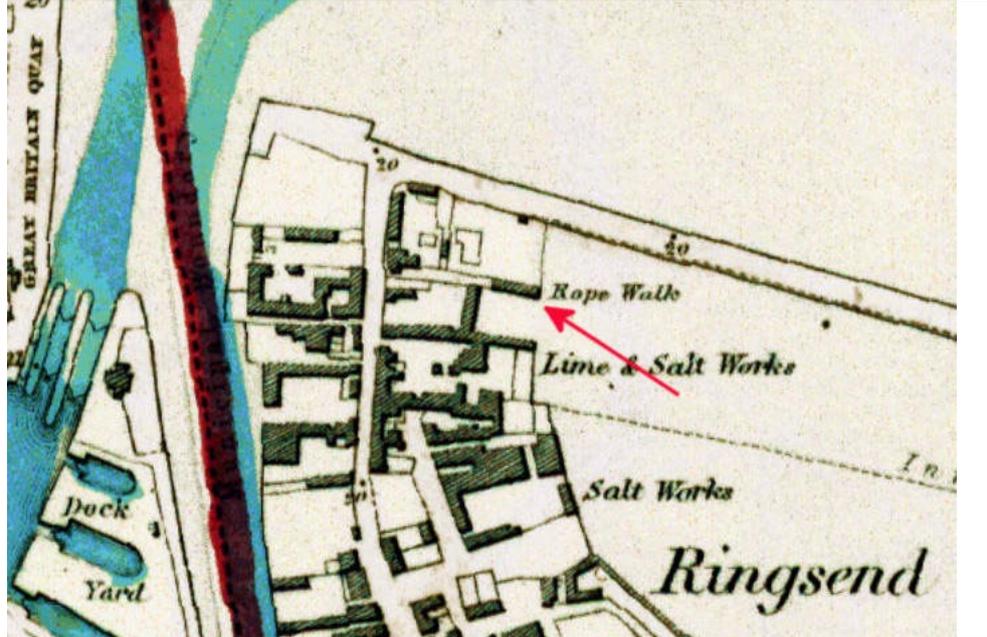
	River Liffey, the landing stage is also close to the Grand Canal and would have provided a vital loading and unloading position for industries in Ringsend.
Image/s	 
Proximity to development	Outside development area
Likely Impacts	None
Recommended Mitigation	None

Reference	Site Name	Site Type	ITM Easting	ITM Northing
DCIHR 18-12-119	Hibernian Works Bottle works	Factory Bottle works	717947	734159
Description	Bottle Works. Glass Bottle making was a major industry at Ringsend. The Hibernian Works Bottle company was owned originally by John Little, and was acquired by John Burke's whiskey distillery in the early 1890s, possibly after an uninsured fire in 1892. ⁵			

⁵ The Tablet, Volume 80 - 1892

	The factory would later become a meat processing plant and was still later occupied by Hammond Metals. An apartment complex now occupies this location (Portview House). Archaeological investigations in advance of the apartment complex recorded reclamation activity dating from the late eighteenth century and associated this with dredging activities led by the Ballast Office and perhaps associated with the construction of the 'South Wall' (00E0744, 00E0744 ext).
Image/s	 <p>Ger Byrne-Ringsend-1940-Hiberian Bottle Works Building</p>
Proximity to development	Outside development area
Likely Impacts	None
Recommended Mitigation	None

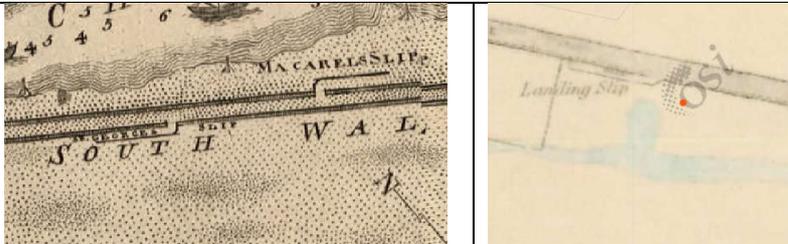
Reference	Site Name	Site Type	ITM Easting	ITM Northing
DCIHR 18-12-149-01	Rope Walk	Building	718043	734112
Description	Variety of nineteenth- and twentieth-century structures occupy the site. Rope Walk building appears on the OS historical 6-inch 1843 as a long thin building using in the manufacture of rope and canvas.			

Image/s	
Proximity to development	Outside development area
Likely Impacts	None
Recommended Mitigation	None

Reference	Site Name	Site Type	ITM Easting	ITM Northing
RMP DU018-053	Ringsend Settlement	Settlement cluster	718006	734002
Description	<p>Ringsend developed as a port for Dublin in the seventeenth century. A bridge was built to Ringsend in 1650 and by 1660 there were 59 inhabitants of English origin and 21 Irish living in the village. A view of Ringsend shows a cluster of buildings around the Church of Ireland erected in 1704.</p> <p>In 1845 Ringsend was described as being in a dilapidated appearance, having fallen into decay since the discontinuance of its extensive salt-works. The same source describes other Industries including Iron Works, Glass making, chemical works and a distillery.</p>			
Image/s				
Proximity to development	Outside development area			
Likely Impacts	None			

Recommended Mitigation	None
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Reference	Site Name	Site Type	ITM Easting	ITM Northing
RPS 7376	Drinking fountain	Drinking fountain	718340	734070
Description	Drinking fountain erected in 1906 with the Ringsend cottages behind the fountain being built in 1922. ⁶ Fountain located at the end of garden wall of 103 Ringsend Park, Dublin 4.			
Image/s				
Proximity to development	Outside development area			
Likely Impacts	None			
Recommended Mitigation	None			

Reference	Site Name	Site Type	ITM Easting	ITM Northing
ADCO 04	Landing Slip	Landing Slip	718700	734008
Description	<p>A landing slip is recorded on John Rocque's map as 'St George's Slip', and on the First Edition OS 6-inch series as 'Landing Slip'. It lay on the south side of the South Wall, and provided access to the channel leading to Cock Lake. The slip is recorded on the OS map as being rectangular in shape with an angle edge on its downstream (east-facing) side. Measured 69m long.</p> <p>The slip is not visible today. Its recorded location places it beneath the community gardens in Ringsend Park and the Syphon House (DCIHR 18-12-151-01). The slip is not referenced in the DCIHR.</p>			
Image/s	 <p>Detail from Rocque 1757 showing St. George's Slip</p> <p>Detail from OS First Edition, with the red dot being DCIHR overlay</p>			
Proximity to development	Adjacent to development area			

⁶ Four Sisters: The History of Ringsend, Irishtown, Sandymount. Kurt Kullmann 2017

Likely Impacts	<ul style="list-style-type: none"> None. The closest development works is a hard landscape zone and tabletop crossing point to Cambridge Ave. junction, which will take place c. 14.5m away.
Recommended Mitigation	<ul style="list-style-type: none"> None.

Reference	Site Name	Site Type	ITM Easting	ITM Northing
DCIHR 18-12-151-01	Syphon house	Syphon House	718758	733985
Description	<p>Detached two-bay single-storey masonry syphon house, built c.1900 as part of the Rathmines and Pembroke Main Drainage Scheme. Now disused. Pitched slate roof with air vent and cast-iron rainwater goods on timber eaves board. Squared roughly coursed rock-faced granite walls with rock-faced limestone quoins. Segmental-headed window openings with block-and-start limestone surrounds and granite sills; now blocked. Square-headed door opening with granite lintel and threshold and block-and-start limestone reveals. The building is one of a group of structures built as part of the Rathmines and Pembroke Drainage Scheme, which was completed in 1881 and was the first main sewage system in Dublin. Situated at the end of a long embankment, the construction of which had a major impact on the geography of Ringsend, the building exhibits an attractive design that belies its functional role. The use of varied stone displaying a mixture of finishings and also skilfully executed, enhances its symmetrical design.</p>			
Image/s				
Proximity to development	Adjacent to development area			
Likely Impacts	<ul style="list-style-type: none"> Incorporated into landscaping of pedestrian crossing to Maritime Village from Ringsend Park. There will be no direct impacts with the Syphon House. The closest development works is a hard landscape zone and tabletop crossing point to Cambridge Ave. junction, which will take place c. 14.5m away. 			
Recommended Mitigation	<ul style="list-style-type: none"> Photographic record Avoid direct impacts 			

Reference	Site Name	Site Type	ITM Easting	ITM Northing
DCIHR 18-12-152	Dublin Corporation Main Drainage Pumping Station	Pumping Station	718866	733934
Description	<p>Dublin Corporation purchased Pidgeon House Fort, which was then in possession of the War Department. With the fort, the Corporation also purchased 60 acres for the purpose of constructing a pumping station and settlement tanks. Dublin Corporation Pumping Station was built in 1906. A 120-foot-high ventilation chimney was one of the striking features of the building. The building badly damaged in an explosion in 1969.⁷ It was replaced in 1985 with a modern pumping station located to the south of the original site. Though the original pumping station is no longer extant, it is possible that power and plant survive within the newer pumping station.</p>			

⁷ RTE Archive 1969

Image/s



Main Pumping Station Ringsend - Dublin City Council Photographic Collection



RTE Archive image of the pumping station following explosion in 1969

Proximity to development

Outside development area

Likely Impacts

None

Recommended Mitigation

None

Reference	Site Name	Site Type	ITM Easting	ITM Northing
DCIHR 19-09-001	Boat slip	Boat slip	719072	733952

Description

Landing slip off the Great South Wall approximately 1km from start of wall. Visible on John Rocque's map dated 1757 where it is referred to as 'Macerals Slip'. Referred to as 'Landing Slip' on the OS Historic 6 inch and as 'Boat Slip' in the OS 25 inch.

The coastguard station c. 1868 (RPS 6792) on Pigeon House Road probably used this slip. Located at the east end of Pigeon House Road opposite Ringsend Sea Scouts den, the slipway may still lie beneath the green verge abutting the MTL Yard.

A reference to the South Wall in 1791 describes three wharfs between the watch tower and the block wharf and seven more between it and the lighthouse.



Detail from Rocque 1757 showing Mackarel's Slip. St. George's Slip is also recorded to the west



Detail from historic OS 25-inch map showing Boat slip DCIHR 19-009-001, and its relationship to the Coastguard Station on Pigeon House Road

	
	<p>The location of the slip is recorded in this aerial image dated early 1930s. The slip was buried during the construction of the container terminal which became operational in 1974.</p>
<p>Proximity to development</p>	<p>Within development area</p>
<p>Likely Impacts</p>	<p>Adjacent to SPAR in Area K</p>
<p>Recommended Mitigation</p>	<p>Archaeological monitoring of ground disturbance activities, with full archaeological recording and preservation <i>in situ</i> if exposed where possible.</p>

Reference	Site Name	Site Type	ITM Easting	ITM Northing
RPS 6782	House	House	718933	733951
<p>Description</p>	<p>70 Pigeon House Road, Dublin 4. This house was named Sandefjord, and site was built in c. 1896, it has a similar design to the Coastguard Station at 80 Pigeon House Road, complete with a lookout tower. The building was not part of the earlier coastguard station houses. Built by George Prescott it was sometimes referred to, by him, as Graphic Cruisers Club. The building, Sandefjorde does appear named on the OS 25-inch map. This property is on a site between the coastguard station and Dublin Corporation Main Drainage Pumping Station (DCIHR 18-12-152). A small slip was located at the southern end of this property. The original property is now truncated by the R131 and the slip, if it still exists, lies beneath the site of Ringsend Recycling.</p>			

Image/s	
Proximity to development	Adjacent
Likely Impacts	Anticipated reduction in noise due to changed use profile of DPC container area opposite. Likely increase in recreational traffic using new maritime centre. No physical impacts
Recommended Mitigation	None

Reference	Site Name	Site Type	ITM Easting	ITM Northing
RPS 6783	House	House	718955	733949
Description	71 Pigeon House Road, Dublin 4. Built c. 1860s, 1 of 10 houses making up the coastguard station. Formerly its address would have been No.1 Coastguard Station, Pigeon House Road. Image shows 71 and 72. The boundary wall in the front garden may be the southern parapet of the GSW.			
Image/s				

Proximity to development	Adjacent
Likely Impacts	Anticipated reduction in noise due to changed use profile of DPC container area opposite. Likely increase in recreational traffic using new maritime centre. No physical impacts
Recommended Mitigation	None

Reference	Site Name	Site Type	ITM Easting	ITM Northing
RPS 6784	House	House	718960	733948
Description	72 Pigeon House Road, Dublin 4. Built c. 1860s, 1 of 10 houses making up the coastguard station. Formerly its address would have been No.2 Coastguard Station, Pigeon House Road. The boundary wall in the front garden may be the southern parapet of the GSW.			
Image/s				
Proximity to development	Adjacent			
Likely Impacts	Anticipated reduction in noise due to changed use profile of DPC container area opposite. Likely increase in recreational traffic using new maritime centre. No physical impacts			
Recommended Mitigation	None			

Reference	Site Name	Site Type	ITM Easting	ITM Northing
RPS 6785	House	House	718964	733947
Description	73 Pigeon House Road, Dublin 4. Built c. 1860s, 1 of 10 houses making up the coastguard station. Formerly its address would have been No.3 Coastguard Station, Pigeon House Road. The boundary wall in the front garden may be the southern parapet of the GSW.			

Image/s	
Proximity to development	Adjacent
Likely Impacts	Anticipated reduction in noise due to changed use profile of DPC container area opposite. Likely increase in recreational traffic using new maritime centre. No physical impacts
Recommended Mitigation	None

Reference	Site Name	Site Type	ITM Easting	ITM Northing
RPS 6786	House	House	718969	733946
Description	74 Pigeon House Road, Dublin 4. Built c. 1860s, 1 of 10 houses making up the coastguard station. Formerly its address would have been No.4 Coastguard Station, Pigeon House Road. The boundary wall in the front garden may be the southern parapet of the GSW.			
Image/s				
Proximity to development	Adjacent			
Likely Impacts	Anticipated reduction in noise due to changed use profile of DPC container area opposite. Likely increase in recreational traffic using new maritime centre. No physical impacts			
Recommended Mitigation	None			

Reference	Site Name	Site Type	ITM Easting	ITM Northing
RPS 6787	House	House	718973	733944

Description	75 Pigeon House Road, Dublin 4. Built c. 1860s, 1 of 10 houses making up the coastguard station. Formerly its address would have been No.5 Coastguard Station, Pigeon House Road. The boundary wall in the front garden may be the southern parapet of the GSW.
Image/s	
Proximity to development	Adjacent
Likely Impacts	Anticipated reduction in noise due to changed use profile of DPC container area opposite. Likely increase in recreational traffic using new maritime centre. No physical impacts
Recommended Mitigation	None

Reference	Site Name	Site Type	ITM Easting	ITM Northing
RPS 6788	House	House	718978	733944
Description	76 Pigeon House Road, Dublin 4. Built c. 1860s, 1 of 10 houses making up the coastguard station. Formerly its address would have been No.6 Coastguard Station, Pigeon House Road. The boundary wall in the front garden may be the southern parapet of the GSW.			
Image/s				
Proximity to development	Adjacent			

Likely Impacts	Anticipated reduction in noise due to changed use profile of DPC container area opposite. Likely increase in recreational traffic using new maritime centre. No physical impacts
Recommended Mitigation	None

Reference	Site Name	Site Type	ITM Easting	ITM Northing
RPS 6789	House	House	718982	733942
Description	77 Pigeon House Road, Dublin 4. Built c. 1860s, 1 of 10 houses making up the coastguard station. Formerly its address would have been No.7 Coastguard Station, Pigeon House Road. The boundary wall in the front garden may be the southern parapet of the GSW.			
Image/s				
Proximity to development	Adjacent			
Likely Impacts	Anticipated reduction in noise due to changed use profile of DPC container area opposite. Likely increase in recreational traffic using new maritime centre. No physical impacts			
Recommended Mitigation	None			

Reference	Site Name	Site Type	ITM Easting	ITM Northing
RPS 6790	House	House	718986	733941
Description	78 Pigeon House Road, Dublin 4. Built c. 1860s, 1 of 10 houses making up the coastguard station. Formerly its address would have been No.8 Coastguard Station, Pigeon House Road. The boundary wall in the front garden may be the southern parapet of the GSW.			

Image/s	
Proximity to development	Adjacent
Likely Impacts	Anticipated reduction in noise due to changed use profile of DPC container area opposite. Likely increase in recreational traffic using new maritime centre. No physical impacts
Recommended Mitigation	None

Reference	Site Name	Site Type	ITM Easting	ITM Northing
RPS 6791	House	House	718990	733940
Description	79 Pigeon House Road, Dublin 4. Built c. 1860s, 1 of 10 houses making up the coastguard station. Formerly its address would have been No.9 Coastguard Station, Pigeon House Road. The boundary wall in the front garden may be the southern parapet of the GSW.			
Image/s				
Proximity to development	Adjacent			
Likely Impacts	Anticipated reduction in noise due to changed use profile of DPC container area opposite. Likely increase in recreational traffic using new maritime centre. No physical impacts			

Recommended Mitigation	None
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Reference	Site Name	Site Type	ITM Easting	ITM Northing
RPS 6792	House	House	718999	733940
Description	<p>80 Pigeon House Road, Dublin 4. House, including former coastguard premises. Built c. 1860s, 1 of 10 houses making up the coastguard station. Formerly its address would have been No.10 Coastguard Station, Pigeon House Road. A boat house is contained within the grounds of this property. The boundary wall in the front garden may be the southern parapet of the GSW.</p> <p>Samuel Carr was promoted to Chief Officer in 10/05/1867 and appointment to Ringsend in June 1870 and June 1879. Thom's Irish Almanac and Official Directory 1870, records the buildings addresses were given as York Terrace, Pigeon House Road, 1 to 10 Coastguard Station. Reference to coastguard station at Pigeon House Road from 1868.</p>			
Image/s				
Proximity to development	Adjacent			
Likely Impacts	Anticipated reduction in noise due to changed use profile of DPC container area opposite. Likely increase in recreational traffic using new maritime centre. No physical impacts			
Recommended Mitigation	None			

Reference	Site Name	Site Type	ITM Easting	ITM Northing
RPS 6793	Fever Hospital, former	Building	719662	733801
Description	<p>The Isolation Hospital was first named the Allan R Ryan Hospital for Consumption, after its founding benefactor, and was officially opened on 23rd August 1910 to treat Tuberculosis. It consisted of a two-storeyed red brick building, which by 1918 came under the care of the Sisters of Charity of St Vincent de Paul, who had a chapel added to the complex.⁸ The site is now occupied by All Away Waste.</p> <p>A later building was constructed to the west of the hospital and within the compound where the boundary wall forms the northern perimeter. The building is recorded as St Catherine's Annexe, and it occupied the eastern side of the present compound. No standing buildings exist on the site today</p>			

⁸ Alan Carthy, 'The treatment of tuberculosis in Ireland from the 1890s to the 1970s, a case study of medical care in Leister', PhD thesis, NUI Maynooth, 2015, pp 167–173.

	A set of steps that predate the hospital are located 100m to the east on the north side of the wall. The gap in that parapet that provided access to the steps has been closed.
Image/s	 <p>Dublin Isolation Hospital – DCC Photographic Collection</p>  <p>View looking southwest along boundary wall in front of the remains of the Fever Hospital</p>
Proximity to development	Adjacent to development area
Likely Impacts	No physical impact. Anticipated increase in HGV traffic along Pigeon House Road
Recommended Mitigation	None

2. Pigeon House Harbour, Fort and Precinct

Reference	Site Name	Site Type	ITM Easting	ITM Northing
RPS 6797; DDIAS 181.1	Pigeon House Harbour, walls and basin	Harbour	720185	733792

<p>Description</p>	<p>Pigeon House Harbour as planned in 1791 formalised a landing place that had developed on the natural islet known as the Green Patch since the late seventeenth century. The harbour forms a large L-shaped area defined by a stone-built quay that measures 335m long east-west by 95m long north-south, and was constructed to serve the packet ferry services. De Courcy argued that the Ballast Office Wall formed the south and east quays, while the west and north quays that project into the river channel date from 1791.⁹</p> <p>When the military requisitioned Pigeon House Precinct in the late 1790s, the harbour's function was redirected to serve military needs and certain adaptations were made to incorporate defensive elements (see description of Pigeon House Fort, DU019-027, RPS 6794).</p> <p>A Lifeboat House (DCIHR 19-09-005) was built inside the north quay of the harbour close to the harbour entrance.</p> <p>When the military departed the precinct and it was bought back by Dublin Corporation in 1897, the harbour was adapted for use and the large western part of its basin filled in with a series of concrete-formed tanks that served as Outfall Tanks (DCIHR 19-09-001) for the city's main drainage scheme. A large culvert that breaches the bottom of the north-facing quay wall is associated with the installation of the Outfall Tanks.</p> <p>The active harbour basin is reduced to a small area at the harbour entrance. From 1902, construction of the Electricity Works complex (RPS 6796) absorbed the eastern side of the harbour basin, and a timber wharf was constructed along this side to provide access.</p> <p>In c. 1982, Dublin Corporation built the Sludge Jetty against the north-facing quay wall.</p> <p>In 1996 the channel area to the north of the harbour was reclaimed and was formerly a recorded oyster fishery for Dublin City.¹⁰</p> <p>An archaeological walkover survey conducted in 2018 at Low Water recorded the west-facing harbour wall as a rough stone-faced wall footing that has had more recent construction built above it. It is possible to trace the line of the former Pigeon House Harbour quay on the ground surface of the reclaimed land to the north, where two parallel lines of stonework are evident, representing the internal-facing and the seaward-facing sides of the former quay. The two lines of stonework lead eastwards to the active harbour basin, where the quay wall is fully exposed. Within the active basin of the harbour, the quay is defined as a well-built granite-block wall.</p> <p>Underwater archaeological survey for the 3FM project recorded a short (c. 50m) section of fully exposed (internal) quay wall along the southeast corner of the harbour area (Appendix 16-5, Feature F03). The quay wall that formed the east side of the historic harbour area is largely obscured by a sheet-pile wall (northernmost part) and a partially collapsed concrete quay. Construction of the sludge jetty has impacted the upper part of the quay wall (along a 15m section) and the culvert for the adjacent Outfall Tanks has removed a 4m by 5m section of the structure's façade.</p> <p>A c. 37m-long section of the east-west quay wall remains fully visible/intact, positioned to the east of the sludge jetty where it forms a pier head at the harbour entrance. Neat-cut, granite, capping adorns the top of the quay-wall along this section, laid north-south to form the surface (deck level) of the quay; ranging in length between 1.1m (min.) and 165m (max.). The vertical face of the capstones measures 250mm in width (horizontal dimension) and 400mm in depth (vertical dimension). The quay wall is composed of uniform courses of granite ashlar, measuring 300 mm in depth (vertical dimension) and between 550 mm (min.) and 1.07m (max.) in length. Fifteen courses of masonry are visible forming the façade at Low Water, upstanding to a maximum height of 4.48m from the foreshore. The masonry courses retain a stepped-profilé, in keeping with the construction of the GSW to the east, with each outward step in the course measuring 30mm, 50mm, or 70mm in width.</p> <p>The pier-head measures 8.4m in width and is flanked by opposing flights of river access steps, positioned on the north (external) and south (internal) side of the structure. The</p>
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⁹ De Courcy, *Liffey in Dublin*, p. 299.

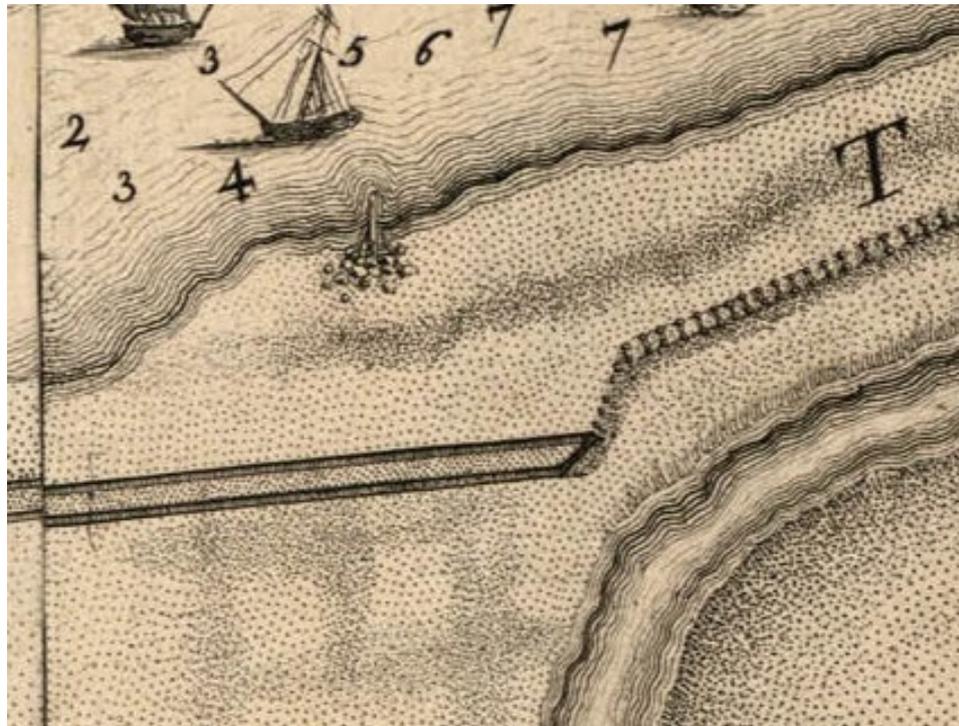
¹⁰ The records survive among the Expired Leases 1462-1869, that form part of the Dublin City Archives, and date to 1705, 1790, 1795, 1799, 1803 and 1832, references exp/1252, exp/1151a, exp/1151, exp/1152b, exp/1152, and exp/1152c respectively

channel side access steps are only partially intact, the topmost seven steps remaining *in situ*. These are formed by masonry that has been set into the quay's façade, the steps extending from the quay wall by 1.24m. The internal steps remain better preserved, extending the full height of the quay wall.

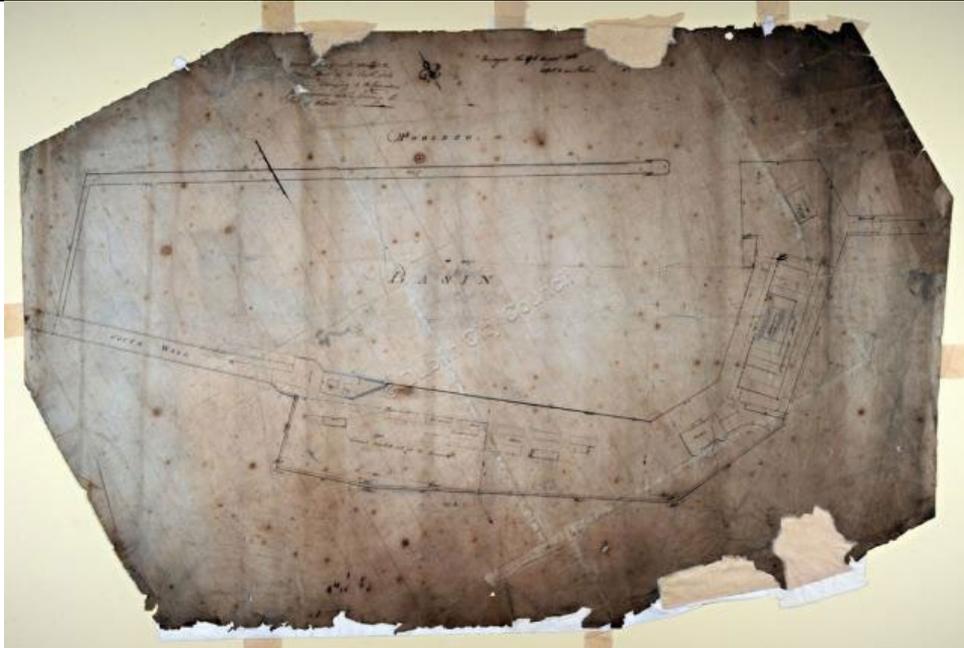
Two mooring bollards are inset into the upper surface of the quay. Both are of an identical (mushroom-cap) design, forming robust cast-iron bollards that stand 500mm from the quay's deck-level. The moorings, which are also located elsewhere within the existing harbour area, are a later addition and are contemporary with the development of the Outfall Tanks.

The east side of Pigeon House Harbour has been substantially altered from the original, where a c. 50m-long section of quay wall (oriented north-south) has been removed and replaced with a sheet-piled wall, set back up to 15m from the original. Extensive rock-armour protection has been placed along the base of quay wall. The timber wharf is in a state of dereliction.

Image/s



Detail from Rocque's map of 1757 showing the area between the Piles (on the right) and the South Wall (on the left) where Pigeon House Harbour was subsequently developed. The slight kink between the two breakwater elements is where the southeast angle of the later Fort is nestled



Plan of Pigeon House Harbour. Source: wsc-maps-312.preview_DubCityPubLibs



Historic painting showing Pigeon House Harbour, and citizens and soldiers travelling along the GSW to the Fort. Note the height of the parapet walls and the presence of the stockaded entrance to the Fort



View looking south at northwest corner of Pigeon House Harbour wall, built c. 1791



View looking west across reclaimed land, showing the line of the former harbour wall still evident on the ground surface



View looking south at north-facing elevation of Pigeon House Harbour wall



Detail view of north-facing elevation of harbour wall showing the stepped profile that is typical of the construction of the GSW.

	 <p data-bbox="408 916 1342 972">View looking east across current basin of harbour at ruinous wharf in front of the former Electricity Works (RPS 6796)</p>
Proximity to development	Within development area
Likely Impacts	<ul style="list-style-type: none"> • Demolition of sludge jetty that abuts harbour wall • Constraints imposed by proposed turning circle and Area N on its use as a harbour
Recommended Mitigation	<ul style="list-style-type: none"> • Detailed supplementary archaeological survey prior to undertaking • Rehabilitation of harbour wall stonework where sludge jetty will be removed as advised by conservation architect/engineer

Reference	Site Name	Site Type	ITM Easting	ITM Northing
RMP DU019-027, RPS 6794	Blockhouse	Building	720140	733719
Description	<p>According to De Courcy, a blockhouse was built in 1760 in the Pigeon house precinct.¹¹ The blockhouse was located close to what would be the western entrance into the Fort (RPS 6794). The blockhouse was used for the storage of tools and materials and as a repository for flotsam and jetsam claimed by the Corporation. In 1761, John Pidgeon became its resident supervisor and caretaker. Many of the people passing through here would stop for refreshments at the resting place established by Pidgeon and his family, and the blockhouse became known as the Pigeon House. The building was timber built and was recorded as being 7,938 feet from the king's Watch House at the Point (Ringsend) to the block-house.¹² New blockhouse accommodation was developed in 1787, when the existing structure appears to have been extended. The blockhouse does not survive above ground today.</p>			
Image/s	None			
Proximity to development	Adjacent to development area			
Likely Impacts	No physical impacts. Road and traffic use will alter character			
Recommended Mitigation	Archaeological monitoring and resolution of all ground works in the vicinity			

¹¹ De Courcy, *The Liffey in Dublin*, 1996, 299).

¹² Encyclopaedia Britannica; Or, a Dictionary of Arts, Sciences – 1791,

Reference	Site Name	Site Type	ITM Easting	ITM Northing
RPS 6795	Former Pigeon House Hotel, Pigeon House Rd	Building	720343	733686
Description	The Pigeon House Hotel is a significant building dating from the eighteenth century. It was built c. 1793 to the design of Robert Pool and constructed to provide accommodation for cross-channel passengers, replacing the former blockhouse. During 1798 the Pigeon House Hotel became part of the military fortifications and remained part of the fortifications until 1897. It currently serves as office space.			
Image/s				
Proximity to development	Adjacent to development area			
Likely Impacts	<ul style="list-style-type: none"> No physical impacts, New perimeter boundary treatment between hotel and public road 			
Recommended Mitigation	<ul style="list-style-type: none"> Archaeological monitoring and resolution of all ground works in the vicinity 			

Reference	Site Name	Site Type	ITM Easting	ITM Northing
RMP DU019-027, RPS 6794	Pigeon House Fort	Building	720133	733678
Description	<p>Shortly before the outbreak of the 1798 Rising, the Pigeon House precinct was requisitioned from the Ballast Board for use as a temporary military fort, which would last until 1897.¹³ According to Kerrigan, the Fort was intended to serve as a citadel for Dublin and as a refuge, with its own harbour for the Dublin administration and garrison.¹⁴ Military reports earlier in the eighteenth century had stressed the need for a citadel, with Ringsend as a suggested site: the establishment of the Pigeon House Fort followed on the same principle of an isolated strong-point with its own harbour.</p> <p>The Fort was entered from a fortified west gate defended by two draw bridges, and extended around the southern and eastern sides of the Harbour. The ground plan is recorded in various maps, including the OS First Edition 6-inch series, and is annotated by De Courcy. The curtilage is recorded as being fortified with two gun embrasures facing south. The Parade Ground lay inside the west gate, extending across the site of the former blockhouse, and a prison house and barracks lay to the south. The Officers Quarters took over use of the Hotel. Other buildings occupied the space to the north, including clerical offices stores and a magazine, with a Hospital located close to the harbour entrance. A Draw Bridge led from the Fort to the eastern extent of the GSW (DU019-029002), next to a Landing Slip, both overlooked by a protective gun parapet. The ferry packet services ceased within the harbour, which now was devoted to military matters.</p> <p>Elements of the Fort still exist today, and an archaeological study of the complex in 2009 concluded that the western gatehouse of the fort, in particular, could be converted into a visiting and orientation space for the fort complex.¹⁵ Some of the upstanding components are recorded as individual monuments (Signal Tower DU019-038001), but not all, and much is buried under later development. Paul Kerrigan's description of the Fort published in 1995 is quite comprehensive and excerpts are presented here:</p>			

¹³ De Courcy, *Liffey in Dublin*, 299-301; Gráinne Shaffrey, et al, *Pigeon House Precinct, Conservation Plan and Re-use Study*, report for Dublin City Council, 2011.

¹⁴ Paul Kerrigan, *Castles and fortifications in Ireland 1485-1945* (Cork, 1995), pp 176-178.

¹⁵ Antoine Giacometti, 'Archaeological and architectural survey of the Pigeon House Fort, and 18th century artillery fort on the Poolbeg Peninsula. 09E0259', unpublished report of ArchTech for Dublin City Council, 2009.

Parts of the perimeter wall or rampart on the west and south still survive. The sea originally came up to the wall at high tide. Entrance to the fort on the west was by means of a drawbridge, which was at the position where the present road enters the fort. Immediately to the north is a gun embrasure, the gun platform some feet above the general ground level. South of the drawbridge were two more gun-embrasures in a length of wall now demolished: the south corner of this part of the west front ended in a triangular salient, loopholed for musketry, which no longer survives. The loops in the northern face flanked this advanced part of the west front and the drawbridge. The perimeter of the fort ran eastwards from the musketry salient for a distance of some 40 feet, then southwards about 145 feet. This north-south length of wall survives, with two arched gun-embrasures of regular masonry in the northern section. The guns were placed on the rampart, approached by steps from the interior of the fort; like those in the advanced part of the west front at the fort entrance, they had a field of fire westwards over the approach from Dublin. Of the south wall of the fort about half survives, a length of some 400 feet. Farther east the road crosses the site of the wall; to the east of the road a length of the south wall with three arched gun-embrasures was demolished in the mid-1980s. From this eastern end of the south wall the perimeter of the fort ran northeast and then northwards to the junction with the eastern extension of the South Wall [Kerrigan is referring here to the GSW running east from the Fort to Poolbeg Lighthouse]. The entrance to the fort here was by means of a drawbridge and through the passageway of a two storey gatehouse. North of the gatehouse on the rampart were three guns on traversing platforms, two front-pivoted and the northernmost gun rear-pivoted in a circular emplacement at the northern corner of the fort. These guns had a field of fire to the east and northeast, commanding the harbour wall and the approach of shipping sailing upstream. To the south of the gatehouse were a large magazine and stores. An element of the fort that survives is a wall forming the southern and eastern perimeter of the Pigeon House Basin, where musketry loops in the wall commanded the harbour. In June 1804 there was a detachment of thirty-five artillerymen in a new ordnance barrack and sixty soldiers in a large house in the eastern part of the fort. Another large house was occupied by officers and civilians, and there was a magazine and naval storehouses and a bombproof capable of accommodating 200 soldiers in an emergency; this must have been a vaulted structure of masonry or brickwork to resist shellfire. The guns were seven 24-pounders, three on traversing platforms; one 18-pounder, three 12-pounders and one howitzer on garrison carriages; and four 12-pounders, two 6-pounders and two howitzers on field carriages. There were in addition eight mortars. A parapet had recently been added to several parts of the fort and a stable built for fourteen horses. A plan of the Pigeon House Fort, with the addition of a much larger proposed fort, drawn to accompany a report of a committee of Royal Engineers in Ireland of November 1805, indicates the outline of the fort much as it appears on later plans.

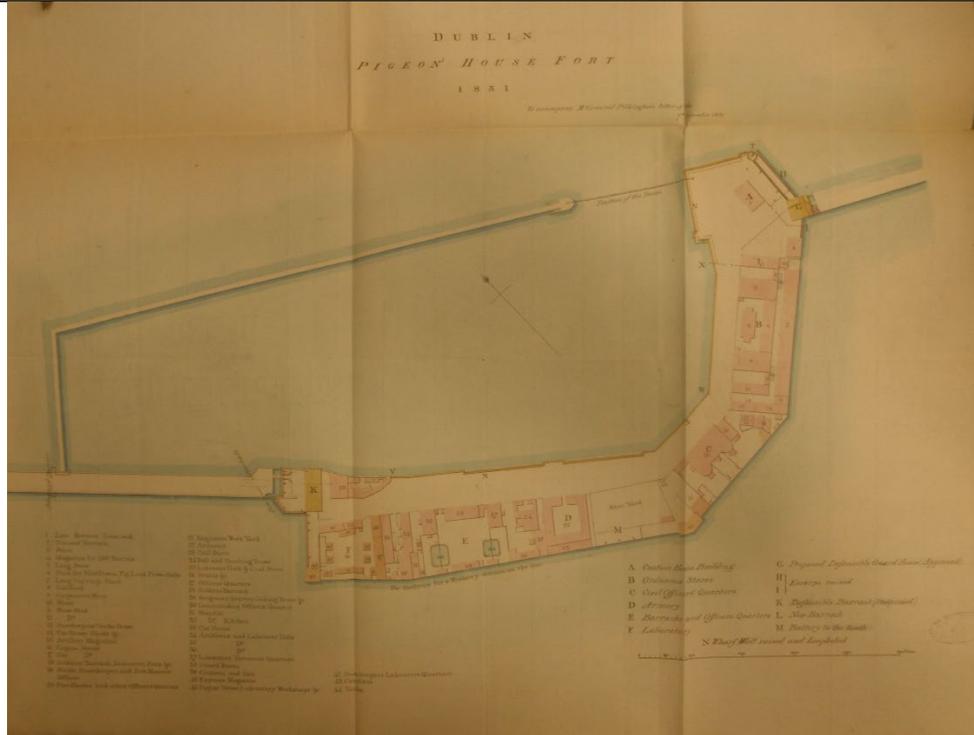
The underwater archaeological impact assessment conducted for 3FM (Appendix 15-5, p. 54) recorded one of the circular gun-emplacements (ADCO STR0-03) located immediately above the quay wall, at ITM 720406E, 733844N (centre-point). The feature is positioned at the terminus of a 'rampart' feature depicted on the OS First Edition Map and as a distinct structure on the OS 25-inch edition map. The structure measures c. 7 m in diameter and is delineated on its seaward circumference by a masonry wall (limestone fabric) comprising six courses of masonry upstanding to a height of 2.9 m. The masonry appears to have undergone repair (historic) in a number of places. Neat-cut, angled, capstones adorn the wall top. Two access steps are visible on its south (landward) side; a third step remaining buried within the modern surface of the quay at this location. A modern concrete structure has been built within STR-03 and is thought to be a pillbox (guard-post) of twentieth-century date.

In addition, the dive survey reaching east of the fort along the base of the GSW observed three concreted iron spheres (probable cannon balls) at ITM 720480E, 733813N (Find Nos. 23D0037:01-03), and provide a tangible reminder of the military development of the Fort in the 1800s.

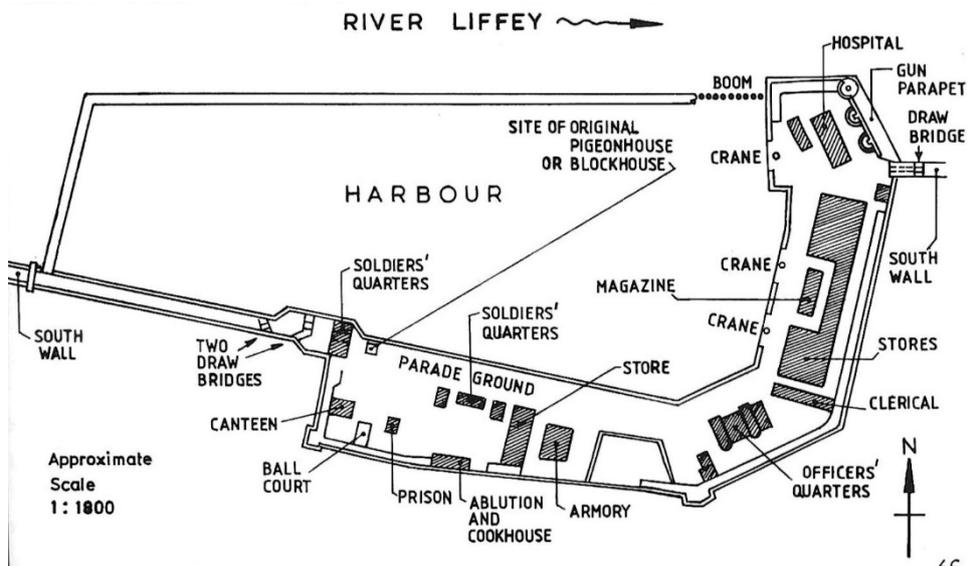
If the Fort is the second principal component of the Pigeon House Precinct following the construction of the Harbour (RPS 6797; DDIAS 181.1), the layered history that is incorporated into the precinct sees further change when the precinct was re-acquired by Dublin Corporation and the site becomes part of the utilities hub that distinguishes Poolbeg Peninsula in the late nineteenth and twentieth centuries. The works associated with the sewerage settlement tanks would infill much of the harbour basin, while those associated with the Power Generation stations from 1902 would both demolish and bury many elements of the Fort. The routing of Pigeon House Road across the site effectively severed the connection between Pigeon House Precinct and the eastern section of the GSW. It also cut the Fort's southern extent in two, with the former barracks and associated buildings and

curtilage lying to the south as an un-used grassy area in what are now lands owned by Uisce Éireann,

Image/s



Plan of Pigeon House Fort, 1831. Source: [Shaffrey Architects](#)



Annotated Plan for Fort c. 1861, by De Courcy, 1996



View looking south-southeast along current west-facing wharf of Pigeon House Harbour. The wharf is supported by a mass concrete quayside that was likely added to the original line of the GSW when constructing the generating station.



Historic photograph showing the western entrance to the Fort. Image courtesy of Cormac Lowth



View looking northeast to the remains of the western entrance today, much of which is now. Only the very northern gatepost of the entrance is standing. The area is planted to provide a degree of landscaping



In contrast, the area of the fort that lies south of the Pigeon House Road and would have included the barracks and the southern perimeter wall is effectively abandoned under rough secondary growth that has developed over what appears to be heaps of dumped soil, on lands owned by Uisce Éireann



Historic photograph showing a selection of the gunports within the Fort. Image courtesy of Cormac Lowth



View looking west along Pigeon House Road that is cut through the Fort, across the former Parade Ground. The stone wall on the right is part of the fortified elements of the GSW that would have overlooked the harbour basin and are furnished with a series of gun loops



Historic photograph showing the eastern entrance to the Fort. Note the GSW leading directly from there, and the Landing Slip to the right (DCIHR 19-09-012). Image courtesy of Cormac Lowth



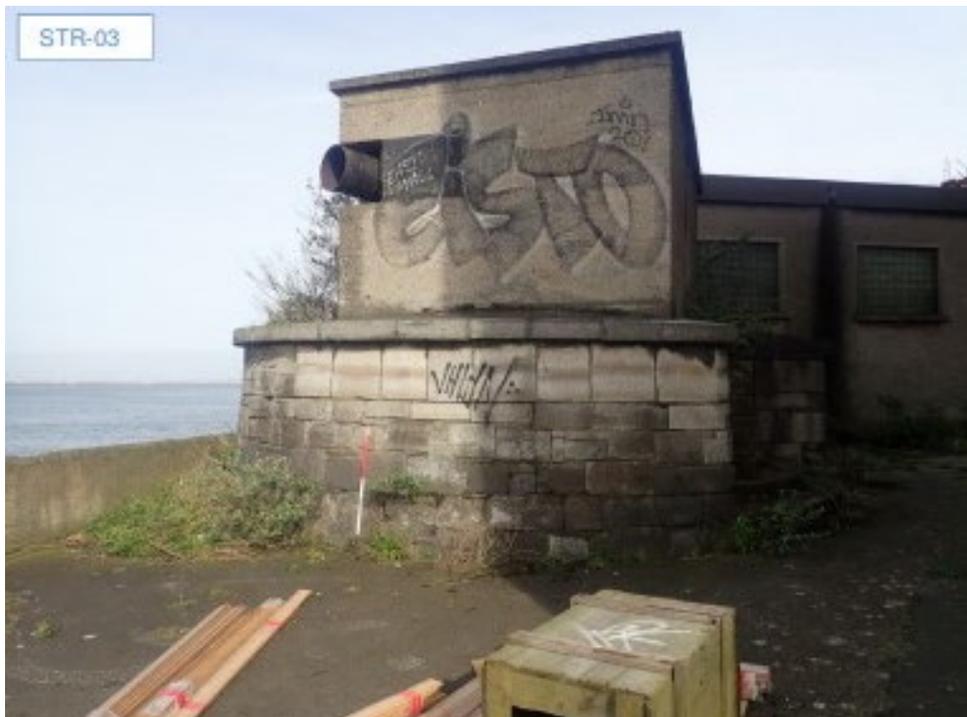
Looking westward towards the eastern entrance to the Pigeon House Fort as seen today



Photograph showing a pair of the Pigeon House Fort guns that formerly served there. Image courtesy of Cormac Lowth



Photograph showing the former gun emplacement located , ADCO STR-03, above the harbour wall. The concrete structure built within the gun emplacement is thought to be a pillbox (guard-post) of twentieth-century date. Image courtesy of Cormac Lowth



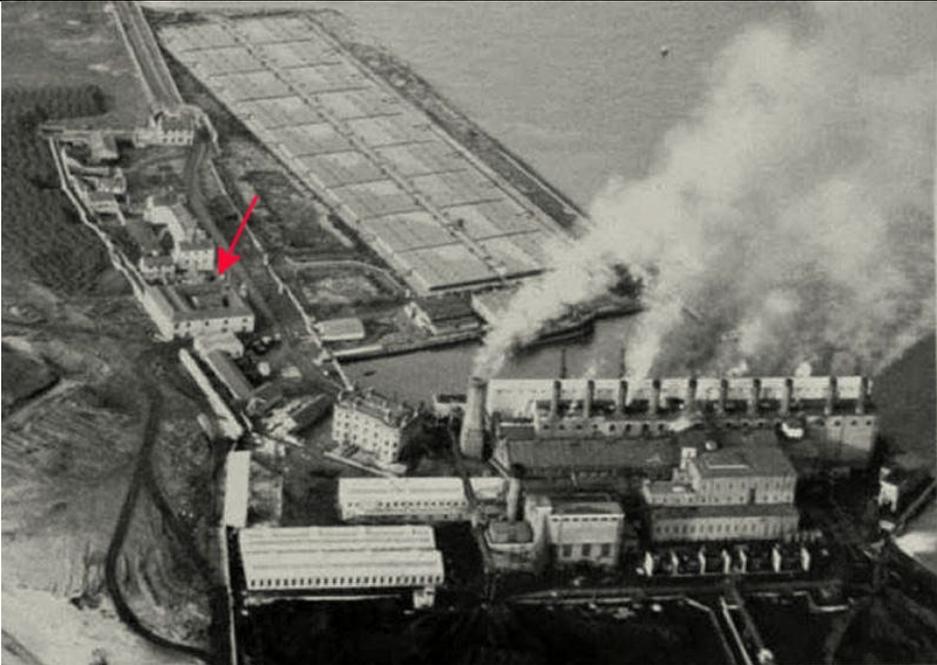
Photograph from UAIA (Appendix 15-5, Plate 51, showing ADCO STR-03 today, where it is subject to graffiti and general neglect

Proximity to development	Within Development area, see Figure 1.2 in EIAR
Likely Impacts	<ul style="list-style-type: none"> • Landscaping to verge of existing road with new perimeter boundaries • New road markings and surfacing with proposed roundabout on location of SE perimeter of fort • Interventions to provide for road safety measures will alter character • SPAR road to access proposed Area N at eastern entrance to Fort • Active travel connection to Area N will traverse along eastern perimeter of fort
Recommended Mitigation	<ul style="list-style-type: none"> • Detailed archaeological survey prior to undertaking • Appropriately scaled and detailed drawings of proposed Area N bridge interaction with quay wall • Designed to avoid direct and indirect impacts with all standing elements of the Fort <p><u>At Western Gate:</u></p> <ul style="list-style-type: none"> • Change in surface and creation of raised table at location of western gate to mark entry into the precinct. • Interpretative/ wayfinding proposal to be provided in this location • Archaeological monitoring of demolition and excavation of below ground elements • General repair works to the GSW in DPC ownership • Site specific boundary treatments which recognize the character of this location <p><u>At Eastern Gate:</u></p> <ul style="list-style-type: none"> • Change in surface and creation of raised table (bridge) at location of eastern gate to mark entry into the precinct. • Archaeological monitoring of demolition and excavation of ground and seabed works. • General repair works to the GSW in DPC ownership.

Reference	Site Name	Site Type	ITM Easting	ITM Northing
RMP DU019-038001	Signal Tower	Signal Tower	720215	733670
Description	<p>In 1804 a signal station was established at Pigeon House Fort. A signal mast is likely to have been the only new feature constructed specifically for the signal station.¹⁶ The signal crew were likely accommodated in existing buildings on site, perhaps at the now demolished barracks shown on the OS First Edition 6-inch map of 1843, on the south end of the fort complex.</p> <p>The signal station was part of a system of over 80 signal stations constructed by the British Board of Ordnance in the first decade of the nineteenth century to warn of the approach of a French invasion fleet. Signalling between sites was accomplished using a naval signal post. The system formed a continuous chain from Malin Head in Donegal to Dublin Bay. It was abandoned by the mid-1810s when the treat of a French invasion diminished.¹⁷</p>			

¹⁶ Kerrigan, *Castles and fortifications in Ireland*, p. 276.

¹⁷ Kerrigan, *Castles and fortifications in Ireland*, pp 157-166.

<p>Image/s</p>	 <p>Aerial photograph of Pigeon House Precinct in c. 1950. The red arrow points to the location of the Signal Tower (RMP DU019-038001) according to record.</p>
<p>Proximity to development</p>	<p>Adjacent to development area, see Figure 1.2 in EIAR.</p>
<p>Likely Impacts</p>	<ul style="list-style-type: none"> • None. The charted location lies within Irish Water lands and consequently is outside the 3FM project footprint
<p>Recommended Mitigation</p>	<ul style="list-style-type: none"> • Archaeological monitoring and resolution of all landscaping works associated with 3FM in this adjacent area because the exact location of the Signal Tower is not confirmed

Reference	Site Name	Site Type	ITM Easting	ITM Northing
DCIHR 19-09-004	Outfall works, Pigeon House Rd	Settlement tanks	720120	733795
<p>Description</p>	<p>Outfall works, completed in 1906 as part of Dublin's main drainage scheme and occupying the west side of the former Pigeon House Harbour basin. In c. 2003, concrete tanks replaced the original settlement tanks.</p> <p>Following the departure of the army from Pigeon House Precinct in 1897, Dublin Corporation commenced work on a long-awaited Municipal Sewerage Scheme, which was completed in 1906, with a large outfall works at the Pigeon House site. Though not the first such scheme in Ireland, it was amongst the earliest and the site represents a major engineering achievement on the part of Dublin Corporation.</p>			

Image/s



Sewer works under construction 1904



Settlement tanks today

	 <p data-bbox="443 719 1390 770">Aerial view looking south across the reclamation area north of Pigeon House Harbour and across the settlement tanks</p>
Proximity to development	Adjacent to Development area, see Figure 1.2 in EIAR.
Likely Impacts	None
Recommended Mitigation	None

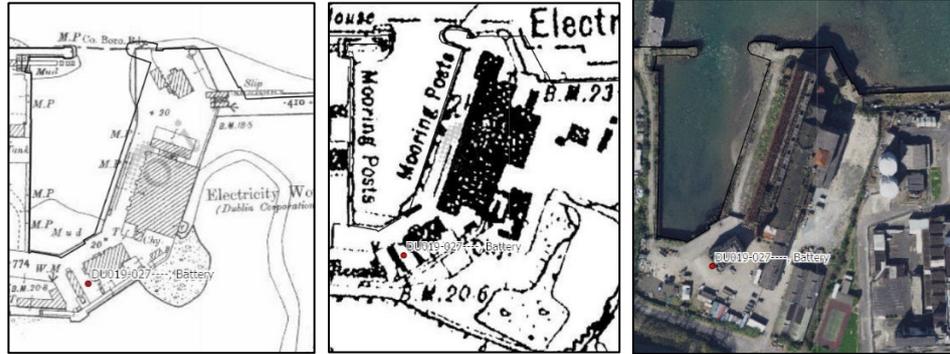
Reference	Site Name	Site Type	ITM Easting	ITM Northing
RPS 6796; DCIHR 19-09-006-01; NCEHD 3271	Electricity Works/ Power Station	Building complex	720398	733755
Description	<p data-bbox="432 1106 1401 1245">The electricity station was built at a time of increased requirement to meet the needs of the rapidly growing city. The station was officially opened in 1902 and in 1903 became the first generating facility in the world to generate three-phase electricity. It was transferred from Dublin Corporation ownership to the newly-created ESB in 1929 and continued to supply electricity to the national grid until it was finally decommissioned in 1976.</p> <p data-bbox="432 1272 1401 1464">It is described as a multiple-bay three-storey red brick former coal-fired power station, with octagonal chimney (rising from square-profile base) attached to south end and cast-iron coal hopper running length of west elevation at roof level. Mid-twentieth-century concrete-rendered extensions to southeast and northeast. Surviving tramlines to yard. Machine-made red brick throughout with variety of square- and round-headed window opes. Palladian style window opening to south gable and metal frame windows throughout.¹⁸</p> <p data-bbox="432 1491 1401 1684">As the country's earliest major power production facility and the world's first three-phase generating station, it is a site of national industrial heritage interest. It is also of considerable architectural significance in its own right and presents what has aptly been described as a 'brick assembly of almost sculptural interest', in particular to its south end, where the combination of gable-front, octagonal chimney and graded massing surmounted by cast-iron coal hopper create a striking set piece. The long east and west elevations also form impressive vistas and the coal-hopper is an important survival.</p> <p data-bbox="432 1711 1401 1796">The later twentieth-century additions are not without some merit also and the whole forms a most impressive composition with the Pigeon House Hotel to its southwest and the chimney's of the new Poolbeg Generating Station (opened 1971) to its east.</p> <p data-bbox="432 1823 1401 1901">Construction of the station initially lay within the footprint of Pigeon House Fort but subsequently was extended east beyond the fort's perimeter wall. It was built over the elements of the Fort in this area. The standing remains do not include the full footprint of</p>			

¹⁸ UCD School of Architecture, Dublin Docklands Area Master Plan: Inventory of the Architectural & Archaeological Heritage, Vol.2, pp.378-79; Brendan Delaney et al, A Heritage Inventory of ESB Buildings in Ireland (2005); Shaffrey, Pigeon House Precinct Conservation Plan, pp 23-33.

the electricity works. Elements along its east side have been demolished, presenting a largely open space. It is this open space that will serve to accommodate the new HGV access road to Area N.

Five site investigations trenches excavated within Pigeon House Precinct along the proposed HGV access road corridor were monitored archaeologically (Appendix 15-4, section 3). The excavation depths achieved were shallow, not exceeding 700 mm, and consistently recorded a surface layer of sand over silt. The silt layer retains inclusions of red brick and metal. Trench 4 included an additional layer of black silt overlying a fine brown-coloured silt. The results indicate the potential for demolished elements of the former electricity works to survive as foundations.

Image/s



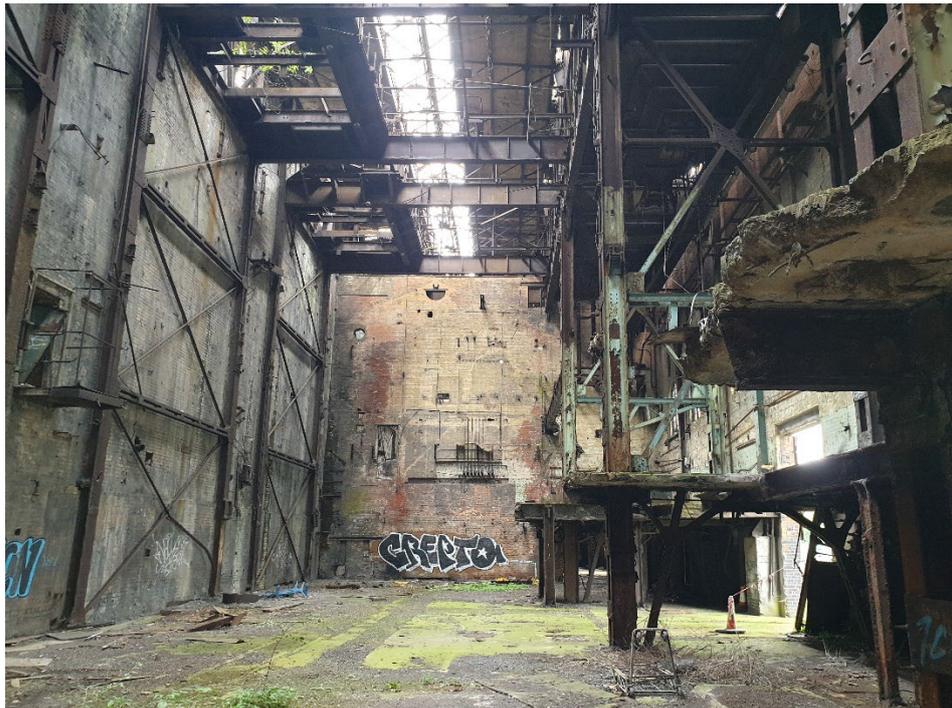
Detail from OS 25inch (c. 1911), OS 3ed Ed (c. 1936) and OS Ortho (2018) showing the footprint of the electricity works through time



View looking north towards the redbrick Electricity Works site with the base of its distinctive octagonal chimney. Pigeon House Hotel (RPS 6795) stands to the right, and the later Pigeon House Chimneys in the background



View looking south along the west-facing façade of the Electricity Works building, and the concrete quayside likely added to the original line of the GSW to facilitate the generating station



The interior of the Electricity Works building is a vast open cavern with a multitude of levels supported by steel frames



View looking west at former electricity works building with later extensions in foreground



View looking west at later extension to electricity works building, north end



View looking south along open space to east of electricity works building, showing large workshop on right side of picture that was added to the southeast corner of the power station c. 1945. Two gable ends of the former GSI laboratories are visible in the middle of the picture. The proposed new access road to Area N requires the demolition of the GSI laboratories and will run north across the open space in this picture

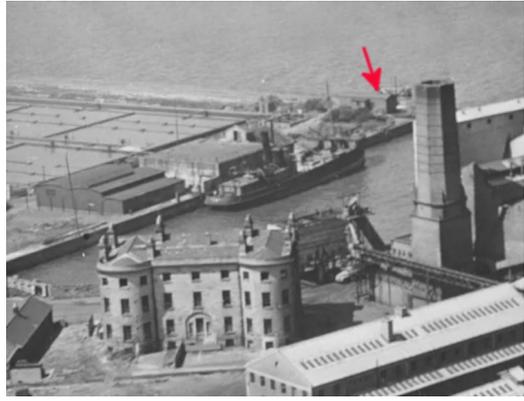


View of eastern gateway from Pigeon House Precinct on to the GSW today. Formerly this retained a draw bridge from the Fort

	 <p data-bbox="432 898 1378 952">View of trough at the eastern entrance to the Fort from the GSW. It is unclear if this was associated with the draw bridge or is a more recent barrier feature</p>
Proximity to development	Adjacent to Development area, see Figure 1.2 in EIAR.
Likely Impacts	<ul style="list-style-type: none"> • Loss of open area to east of former Electricity Generating Station. Proximity of Area N requires protective mitigation measures during construction and operation due to poor condition of former Power Station building. • Change in character of area.
Recommended Mitigation	<ul style="list-style-type: none"> • Construction Management Plan review and approval by Architect/ Engineering conservation • Consideration of fencing design, road surfaces of access road, and access bridge from Precinct to Area N to absorb heritage importance of the Precinct area • Vibration monitoring of protected structure (RPS 6796) during construction of access road and bridge • Archaeological monitoring and resolution

Reference	Site Name	Site Type	ITM Easting	ITM Northing
DCIHR 19-09-005	Lifeboat House, Pigeon House Rd	Building	720278	733835
Description	<p data-bbox="421 1462 1378 1574">In a 1907 edition of 'The Builder' there is an invitation to tender for the construction of a timber and corrugated Lifeboat house and timber slipway upon a site near Pigeon House Fort. This construction would have been on land reclaimed for the settlement tanks for the Corporation's Outfall works.</p> <p data-bbox="421 1597 1378 1686">The DCIHR record suggests that an ashlar masonry pier located to east of site may be related to the lifeboat house. The building and timber slip are no longer visible and were possibly removed when Sludge jetty was installed in the 1980s.</p> <p data-bbox="421 1709 1378 1854">The underwater archaeological impact assessment completed for 3FM (Appendix 15-5) observes that the OS-25 inch map depicts a 'Lifeboat House' and associated launching slip at this location, which is at the western pier head, just east of the settlement tanks. It is likely that the angled retaining-wall represents the remains of a sub-structure to a timber-framed (or similar) superstructure from which a life-boat was launched/ recovered.</p>			

Image/s

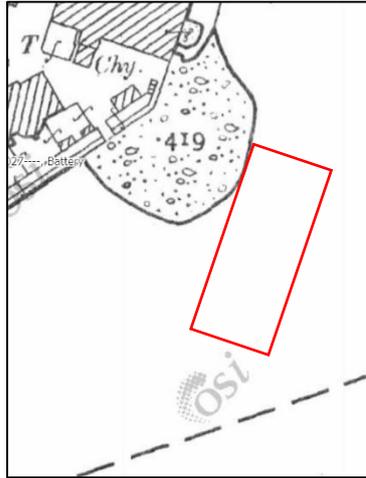


View looking north today at location of former slip that has been infilled

	 <p data-bbox="421 913 1385 965">View looking west today at location of former slip that has been infilled</p>
Proximity to development	Adjacent to Development area, see Figure 1.2 in EIAR.
Likely Impacts	None
Recommended Mitigation	None

Reference	Site Name	Site Type	ITM Easting	ITM Northing
ADCO 03	Sheds/GSI Laboratories	Building complex	720396	733647
Description	<p data-bbox="432 1243 1398 1332">Series of two conjoined single-storeyed building units that served as sheds for the electricity works and subsequently as laboratories for the Geological Survey of Ireland, and were most recently used to support Film studios.</p> <p data-bbox="432 1355 1398 1444">The units were built post-1945 on reclaimed land that lies within the Pigeon House Precinct but outside the footprint of Pigeon House Fort (RPS 6794) and outside the standing remains of the electricity works protected structure (RPS 6796).</p> <p data-bbox="432 1467 1398 1780">An oblique aerial photograph of the electricity works dated c. 1952 shows the units as two long sheds with single gabled low pitched roofs. Since then, the southern half of the sheds have been reroofed and the original windows replaced, dividing the two units into four. The older elements are roofed in either corrugated concrete sheets or asbestos sheeting, and retain two elevated ventilation units and are punctuated with roof lights. The roofs of the newer elements appear to be felt sheets. The walls of the sheds are formed using concrete frames that are infilled with poured concrete wall panels that support window lights and doors. The windows on the older elements are tall sash windows, while those on the newer elements are either aluminium or PVC frames. The west-facing and north-facing exterior façades only were inspected. The interiors were not inspected.</p>			

Image/s



Detail from OS 25-inch map
Showing approx. location of future
sheds as open water c. 1911



Detail from OS Ortho image showing
location of the sheds



Aerial photograph of the electricity works in c. 1952, with the buildings location
highlighted. DPC Archive ref 0519



View looking east at former sheds/GSI laboratory building



View looking northeast at conjoined building of the former sheds/GSI laboratory building

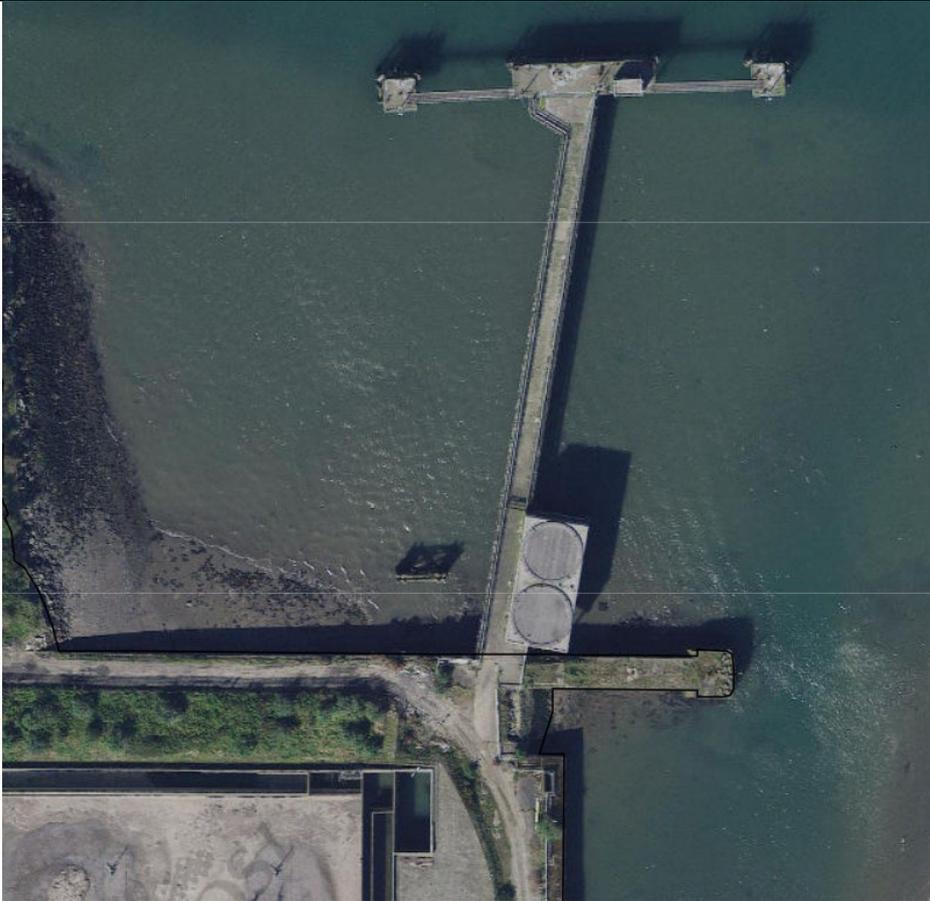


View looking north at conjoined building of the former sheds/GSI laboratory building

Proximity to development	Within development area, see Figure 1.2 in EIAR.
Likely Impacts	<ul style="list-style-type: none"> • The building complex will be demolished to facilitate HGV access road to Area N
Recommended Mitigation	<ul style="list-style-type: none"> • Detailed archaeological survey prior to construction phase • Site specific boundary treatments which recognize the character of this location • Removal of the formalization of the IW/DCC construction access rout • Archaeological monitoring and resolution during construction

Reference	Site Name	Site Type	ITM Easting	ITM Northing
ADCO 02	Sludge Jetty	Jetty	720290	733941

Description	<p>The Sludge Jetty was built by Dublin Corporation in the early 1980s and gets its name from its principal purpose being the loading point for municipal waste generated by the Outfall Works that would be transported on to the <i>Sir Joseph Basildgate</i> at the jetty for disposal at sea.</p> <p>It is a simple functional design based on pairs of steel piles that are furnished with a reinforced concrete lintel which supports the jetty deck. The jetty is wider at its shore end where a series of service rooms are constructed on its downstream side. It was built on to the north wall of Pigeon House Harbour, and its construction impacts extend across the former deck area of the north wall. The underwater archaeology impact assessment for 3FM (Appendix 16-5) recorded the impact area extending over a 15m-long (east-west) extent of the eighteenth-century quay.</p> <p>The jetty is c. 106m long and is orientated north-northeast – south-southwest. The pier head is T-shaped to accommodate berthing vessels for loading purposes and measures c. 72m long east-west.</p>
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Image/s	 <p>Detail OS Ortho image showing the Sludge Jetty and its tie-in to the north wall of Pigeon House Harbour. Note the impact on the harbour wall extends across its surface</p>
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View looking east at Sludge Jetty. The timber structure in the foreground is described below as STR-01 and is separate from the Sludge Jetty



View looking west at Sludge Jetty, with the service buildings in the foreground

	 <p data-bbox="427 902 1331 954">View looking west at where Sludge Jetty is built on to the north wall of Pigeon House Harbour</p>
Proximity to development	Within development area, see Figure 1.2 in EIAR.
Likely Impacts	<ul style="list-style-type: none"> • The Sludge Jetty will be demolished to accommodate the turning circle, and repairs to north wall of Pigeon House Harbour made good
Recommended Mitigation	<ul style="list-style-type: none"> • Detailed archaeological survey prior to undertaking • Archaeological monitoring and resolution during demolition • Building conservation-led rehabilitation and rebuild of north wall of Pigeon House Harbour

Reference	Site Name	Site Type	ITM Easting	ITM Northing
ADCO STR-01	Timber structure	Jetty, site of	720262	733622
Description	<p data-bbox="434 1326 1382 1572">STR-01 was recorded as part of the underwater archaeology impact assessment for 3FM (Appendix 15-5, Figures 29-30). It comprises a timber structure that is positioned c. 14 m north (channel side of the culvert associated with the Pigeon House outfall works. The structure is formed by two square-shaped timber columns, spaced 5 m apart, conjoined by a horizontal timber frame. Each column is composed of four vertically-set timber posts (measuring 300 mm x 300 mm) with a lattice-work of internal cross-beams forming the sides. Each element is fastened together using large (recessed) iron-bolt fastenings. Each of the piles is protected by an iron pile-cap. The overall structure measures 8.32 m east-west by 1.35 m north-south.</p> <p data-bbox="434 1599 1382 1733">The exact function of STR-01 remains unclear. However, it appears to retain some age and is depicted on OS 25-inch Map of the area. Moreover, the adjacent culvert, having undergone modern repair as part of the modernisation of the outfall works in c. 2003, most certainly formed part of the earlier works at this location. It is likely that STR-01 is associated with the original culvert and is of a similar date.</p>			

<p>Image/s</p>	 <p>View looking east at the timber structure STR-01, with the Sludge Jetty in the background</p>
<p>Proximity to development</p>	<p>Within development area, see Figure 1.2 in EIAR.</p>
<p>Likely Impacts</p>	<ul style="list-style-type: none"> • ADCO STR-01 will be demolished in order to facilitate the Turning Circle
<p>Recommended Mitigation</p>	<ul style="list-style-type: none"> • Archaeological monitoring and resolution during demolition

3. GSW from Pigeon House Precinct to Poolbeg Lighthouse

Reference	Site Name	Site Type	ITM Easting	ITM Northing
DU019-029002, DCIHR 19-09-010, RPS 6797, RPS 6798	Sea wall. Great South Wall to Poolbeg Lighthouse	Sea Wall	Various	Various
<p>Description</p>	<p>The first attempt to construct a breakwater that would influence tidal flow in favour of navigable access upper the Liffey channel to the city quays occurred between the Green Patch and Poolbeg Light, and is recorded by Rocque (1757) as the Piles. In 1759, the Ballast Office decided to continue the double masonry wall from the east abutment at Poolbeg to the recently constructed Ballast Office Wall. The construction was by John Smith of Dublin. Progress was slow and only a length of 500m was complete by 1787. Construction was completed 1792-1795.</p> <p>The main wall length has a different construction to that of the Ballast Office Wall, comprising a dry rubble core with granite ashlar in distinctive linear-shaped blocks that form the two façades of the wall and has a 24-foot wide deck surface. This wall length did not have parapets as it was not constructed as a protected roadway. It served as an operational breakwater. The final configuration has lent its name to the whole length that runs from Ringsend out to the Poolbeg Lighthouse as the GSW. At the time it was completed, the GSW was the longest constructed breakwater in the world.</p> <p>In c. 1770, two slipways were added to the GSW east of the Fort. The first (DCIHR 19_09_012_01) was added next to the eastern entrance to the Fort, and the second (DCIHR 19_09_011_01) was added at White Bank, giving access to the Liffey Channel. In 1793, the Half-Moon Battery (DU019-028), a three-gun battery was built further east.</p>			

	<p>Other features were added in the nineteenth century, including the Sluice House (DCIHR 19_09_009_01) and a Life Boat House (DCIHR 19_09_008_01).</p> <p>A letter dated 12/1/1801 details proposals by Captain William Bligh for the repair of the wall from the lighthouse to the battery, involving raising the wall 1.3m higher than the Ballast Office Wall. Mention is also made of additional rock armour to protect the wall. A length of 1,307m was completed by 1803 and the further 1,347m were commissioned thereafter. By about 1805 the Great South Wall as we know it was complete.</p> <p>In 1861, following damage caused by an extreme weather event at Poolbeg Lighthouse that dislodged stonework, Bindon Blood Stoney had a series of rock armour protection measures applied below the waterline.</p> <p>An extreme weather event in 1981 resulted in the exposure of the remains of wooden kishes associated with The Piles on the south side of the GSW, and damage to the decking of the GSW. Dublin Port Company responded by adding further rock armour protection, and such protection is seen now on both sides of the GSW; that on the south side reaches level with the deck area in places, while that on the channel side tends to lie lower. Dublin Port Company carries out regular monitoring of the GSW to ensure its stability, and this monitoring is achieved by annual LiDAR and multibeam surveys, yielding robust metrically accurate data.</p> <p>There have been some archaeological inputs to investigations conducted on this section of the GSW. A programme of Site Investigations (SI) conducted in 2015 permitted insight to its construction and state.¹⁹ Five boreholes were cut through the wall between the Slipway DCIHR 19-09-11 and Poolbeg Lighthouse. The archaeological report concluded that the wall was built as a solid structure using cut granite blocks and gravel or shingle as fill, with some limestone and sandstone evident in the lower layers. Cut granite blocks were set dry on the seabed, flanked by two retaining walls. The seabed surface was sand that reaches depths of up to 10m. The sand in turn overlies strata of gravel, shale and boulder clay. Bedrock was recorded 30-45m below the surface of the wall. In BH3, which lies close to the Half Moon Battery (DU019-028), timber was observed underneath the wall and this was interpreted as the possible remains of wreckage.</p> <p>In 2020, a section of the original revetment and inner and outer walls of the GSW were removed under archaeological supervision to accommodate a pipeline crossing the GSW opposite the NORA tanks, at approximately ITM721031E 733823N.²⁰ Following the insertion of the pipeline, the revetment and walls were reinstated. No other features of archaeological significance were observed during the monitoring works.</p> <p>As part of SI works carried out for the 3FM project (Appendix 15-4, section 3, Trench 7 and BH-212), one location was excavated against the south side of the GSW to inform engineering design for the ESB access bridge that will cross on to Area N. The Borehole recorded sand, gravel and cobble extending to 10.2 m depth on the south side of the wall, while the trench exposed the stepped profile of the wall to a depth of 2 m as anticipated, at which point seawater encroached into the trench. The GSW at this location was recorded as being built of dry rubble-stone, with its surface and sides faced with granite ashlar blocks. The blocks are well bonded, but laid dry, except on the top where the joints are pointed. The sides of both the inner and outer walls are battered; approximately 1 in 7. Each course of granite masonry measured 400 mm in depth, with a horizontal offset of 50 mm from the underlying blockwork.</p> <p>The underwater archaeology impact assessment carried out for the 3FM project (Appendix 16-5) completed measured survey along 990 m of the GSW that lies within the development footprint, and provides detailed and measured descriptions of the exposed surfaces. The supporting drawings include laser-scan detail.</p>
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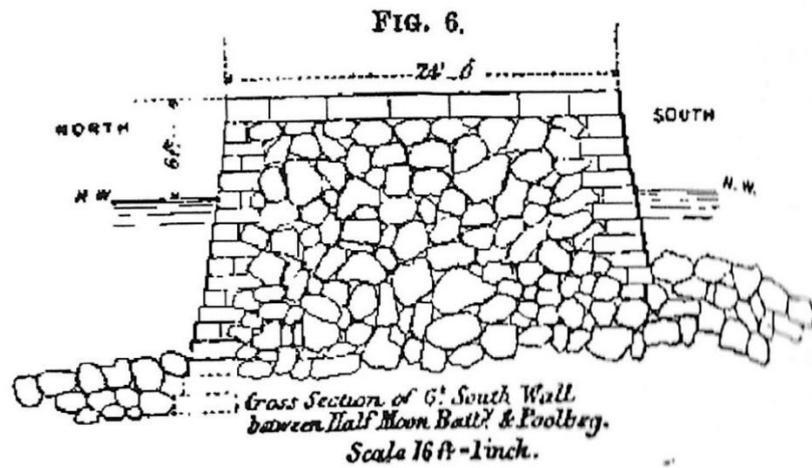
¹⁹ Anon, 'DPC historical structures studies, Great South Wall, Port of Dublin. Site Investigation report', Irish Drilling Ltd, unpublished report for Dublin Port Company 2016; David O'Connor, 'Archaeological monitoring of ground investigations (boreholes) at Great South Wall, Poolbeg, Dublin 2. 15E0454', Magnus Archaeology report for Dublin Port Company.

²⁰ Siobhan Deery, 'Archaeological monitoring at the South Sea Wall, Dublin 2. 19E0654'. Courtney Deery report for Dublin City Council.

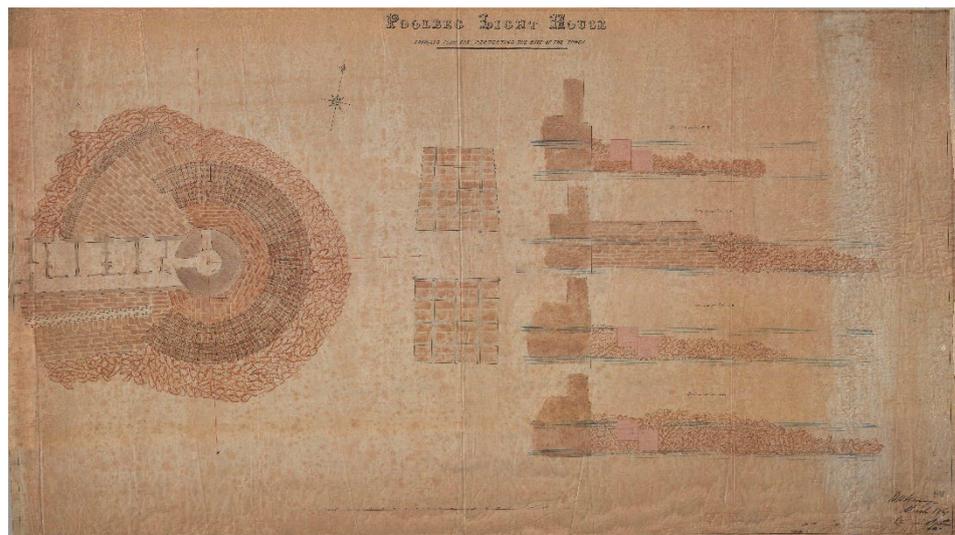
Image/s



Detail from Rocque 1757, showing The Piles.



Cross-section of the Great South Wall recorded in Mann, 1881.



Bindon Blood Stoney engineering plan to show proposed rock armour protection measures to be added to the base of the GSW at Poolbeg Lighthouse in 1861 following destruction wrought by an extreme weather event. DPC Archive 8437



Photograph taken after the storm in 1981 shows damage to the GSW and a line of timbers off the south side, which appear to be the remains of 'The Piles'. Source: Dublin Port Company, Port Engineer



View looking east along GSW today just past White Bank Wharf Slipway ((DCIHR 19_09_011_01). The rock armour protection placed at the base of the wall on the foreshore, and reaching the deck level on the south side are modern interventions



View looking east along GSW today alongside the Lifeboat House site (DCIHR 19_09_008_01). The low concrete barrier supporting the vertical ranging rod is a modern addition



View looking west along GSW at the NORA tanks (left). The new wharf Area N will lie on the channel side (right)

	 <p data-bbox="432 898 1377 958">North-facing view of the southern side of the GSW, recorded during SI works for the 3FM project, Trench 7 (BH-212)</p>
Proximity to development	Within development area, see Figure 1.2 in EIAR.
Likely Impacts	<ul style="list-style-type: none"> • Proposed signal junction at Pigeon House Rd and Shelly Banks Rd • 3 No. proposed crossings above GSW at proposed Area N. These will restrict potential for future access from Pigeon House Fort to eastern section of GSW (currently inaccessible)
Recommended Mitigation	<ul style="list-style-type: none"> • Creation of a physical separation between the new berth and the GSW • Area N designed to be reversible • View corridor to be incorporated along length of inaccessible portion of GSW alongside Area N • New bridge design to be appropriate to the context and designed to be reversible • Design of boundary railings to allow for maximum transparency • Detailed supplementary archaeological survey prior to undertaking • All survey data to be lodged in the Dublin Port Archives • Sample panel for conservation pointing, grouting and reconstruction works to be approved by Heritage team and DCC • Architect conservation inputs into the design elements of the Area N bridges • Architect conservation supervision of the Area N bridges • Archaeological monitoring and resolution of ground and seabed activities • Rehabilitate/ make good the GSW deck surface along its length

Reference	Site Name	Site Type	ITM Easting	ITM Northing
DCIHR 19-09-012	Landing Slip	Landing Slip	720462	733814
Description	<p data-bbox="400 1700 1388 1839">A substantial, masonry boat-slip (DCIHR 19-09-012) adjoins the GSW beside the eastern entrance to Pigeon House Fort. The slipway was constructed c. 1770 and is recorded on both the OS First Edition 6-inch series and OS 25-inch maps. It is contemporary with the construction of the GSW and is catalogued in the underwater archaeological survey completed for 3FM as STR-04 (Appendix 16-5, Figure 37).</p> <p data-bbox="400 1865 1388 1989">The slipway measures 25m long and 4.3m wide. It slopes gently from west to east at a c. 10° angle. The slip's surface comprises rectangular granite masonry, retaining similar dimensions to the blocks used to face the surface of the adjacent GSW. The structure is upstanding to a maximum height of 1.6m from the foreshore, where four courses of granite masonry are visible. The slipway has similar dimensions to the ashlar used in the GSW and</p>			

	also retains a stepped profile. A series sheet piles protrude (max. height of 500mm) from the silty-sand on the channel side of the slipway.
Image/s	 <p>View looking southeast at slipway adjoining GSW outside eastern entrance to Pigeon House Fort</p>
Proximity to development	Within Development area, see Figure 1.2 in EIAR.
Likely Impacts	<ul style="list-style-type: none"> • No direct impacts • Area N main access bridge will traverse above slipway and GSW •
Recommended Mitigation	<ul style="list-style-type: none"> • Avoid impacts direct and indirect • Detailed supplementary archaeological survey prior to undertaking. • All survey data to be lodged in the Dublin Port Archives • Archaeological Monitoring and resolution

Reference	Site Name	Site Type	ITM Easting	ITM Northing
DCIHR 19-09-015	Poolbeg Generating Station, Chimneys	Chimneys	720535 and 720617	733751 and 733752
Description	Twin concrete chimneys of Poolbeg Generating Station that are an iconic symbol of Dublin City's maritime skyline. Built and completed in the 1970s			

Image/s	 <p data-bbox="408 667 1383 723">View looking west towards the Pigeon House Chimneys from the GSW at White Bank Wharf</p>
Proximity to development	Outside development area
Likely Impacts	None
Recommended Mitigation	None

Reference	Site Name	Site Type	ITM Easting	ITM Northing
DCIHR 19-09-011	Slip, White Bank Wharf, South Wall	Slipway	721402	733860
Description	Slip, believed to have been constructed c. 1770 and is recorded on the OS First Edition 6-inch and later maps. The slipway is integrated into the GSW and is close to a flight of access steps located upstream. The stonework on the slipway comprises long lengths of rectangular masonry (measuring between 1m-1.30m long) that have been laid at right angles to the sea wall. The northwest corner of the slipway has suffered erosion and partial collapse. The damage appears to be a direct result of wash from shipping entering/exiting Dublin Port at Low Water			
Image/s	 <p data-bbox="408 1937 1383 1966">View looking east at access steps and slipway, with Sluice House in the distance</p>			
Proximity to development	Outside development area			

Likely Impacts	None
Recommended Mitigation	None

Reference	Site Name	Site Type	ITM Easting	ITM Northing
DCIHR 19-09-008	Lifeboat House	Building	721370	733814
Description	Lifeboat house, c.1850, site of. A lifeboat existed at Pigeon House as early as 1815, although apparently was later transferred to Bullock. ²¹ Standing remains are evident in historical photographs but today only lengths of ruined wall stubs stand above ground level to indicate the presence of buildings. Red brick as well as granite occurs in the surviving fabric. An enclosure wall also survives as an old concrete construction that has been breached in places. The wall stands inside a perimeter path whose concrete surface is also ruinous and reflects dereliction from coastal processes.			
Image/s	 <p>Dublin Port Archives 0259i, detail from historical photograph from 1939 indicating standing remains within the site of the Lifeboat station (on right). The Sluice House (DCIHR 19_09_009) is also recorded to the centre, along with the GSW and Poolbeg Lighthouse</p>  			
Proximity to development	Outside Development area			
Likely Impacts	None			
Recommended Mitigation	None			

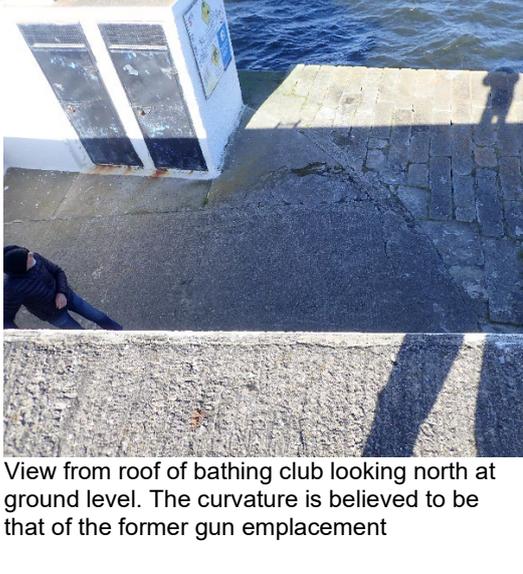
Reference	Site Name	Site Type	ITM Easting	ITM Northing
DCIHR 19-09-009_01	Sluice House, South Wall	Sluice House	721427	733833

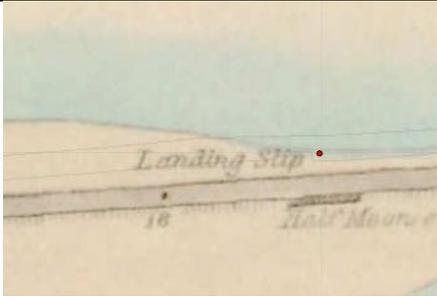
²¹ H.A. Gilligan "A History of the Port of Dublin" - 1988

Description	<p>Detached three-bay single-storey masonry former sluice house, built c.1881, in association with Londonbridge Road Pumping Station, now abandoned. Squared-random rubble stone walls with concrete coping and rusticated quoins. Segmental-headed window openings with rusticated block-and-start surrounds, now blocked. Square-headed door opening with rusticated granite surround and steel door c.1990.</p> <p>The sluice was constructed as part of the Rathmines and Pembroke Main Drainage Scheme, which was completed in 1881. The structure housed machinery to operate two penstock valves which managed sewage flow and the pressure of seawater at the sewer outlet. Though no longer in use, the structure retains much of its original form and features, while its fine stonework attests to the highly skilled engineers and craftsmen involved in the construction of the scheme. This was the first such scheme to be completed in Ireland and as such is of both historical and social, as well as industrial, significance.</p>
Image/s	
Proximity to development	Outside Development area
Likely Impacts	None
Recommended Mitigation	None

Reference	Site Name	Site Type	ITM Easting	ITM Northing
RMP DU019-028	Half Moon Battery Swimming Pool and slipway	Battery	722275	733893
Description	<p>In 1793 the Board of Ordnance built a gun battery on the GSW, c. 800m from the Poolbeg lighthouse. The Half Moon or Five Gun battery was armed with five 24-pounder guns, and it controlled the channel of the river and the water around the Poolbeg lighthouse. The battery was subsequently dismantled, and the site is used as a swimming-place.²² An addition was made to the battery in 1795. Part of the structure survives, but not enough to establish how the guns were arranged.</p> <p>The south-facing perimeter wall is granite-built and is the most intact upstanding element of the former battery. A curvature in the ground surface of the GSW is believed to mark the swivel position of the gun battery.</p> <p>Two ranges of white-washed buildings today serve the swimming club. The more substantial range is on the south side and comprises the Ladies changing rooms to the east, and the Men's changing rooms to the west. The interior of both changing rooms is</p>			

²² Kerrigan, *Castles and Fortifications*, p. 176; De Courcy, *Liffey in Dublin*, p. 185.

	<p>finished in concrete blockwork but the external façades indicate stone work surviving to some height that is likely associated with the battery. The ceiling is a continuous flat concrete pad that is set back from the granite blocks along the south side. The concrete ceiling has cracked and bitumen applied as repairs to fill the cracks but these are now failing with resulting water ingress.</p> <p>The second range of buildings stands on the north side of the site and comprises two small concrete-built and concrete-roofed structures, one of which serves as the office for the swimming club, the other for storage.</p> <p>A stone-built slipway extends westwards from the west end of the battery, and is recorded on the OS First Edition map series.</p>
<p>Image/s</p>	<div style="display: flex; justify-content: space-around;"> <div data-bbox="405 506 839 824">  <p>View from north</p> </div> <div data-bbox="865 506 1372 824">  <p>View from east</p> </div> </div>
	<div style="display: flex; justify-content: space-around;"> <div data-bbox="405 862 839 1180">  <p>View looking west, showing upstanding south-facing wall element of the former battery that now houses the changing rooms for the swimming club</p> </div> <div data-bbox="865 862 1302 1180">  <p>View looking west across the concrete roof of the bathing club, showing bitumen repairs</p> </div> </div>
	<div style="display: flex; justify-content: space-around;"> <div data-bbox="405 1294 839 1868">  <p>Interior view of the Men's changing rooms, showing blockwork interior and concrete roof</p> </div> <div data-bbox="865 1294 1388 1868">  <p>View from roof of bathing club looking north at ground level. The curvature is believed to be that of the former gun emplacement</p> </div> </div>

		
	Detail from OS First Edition map showing extent of slipway associated with the battery	
	View looking west with element of stone slipway showing	
Proximity to development	Outside Development area	
Likely Impacts	None	
Recommended Mitigation	None	

Reference	Site Name	Site Type	ITM Easting	ITM Northing
RPS 7379	Lighthouse	Lighthouse	723102	734025
Description	<p>The provision of a navigation aid where the River Liffey maintained a natural gap across the Dublin Bar was initially satisfied by the presence of a buoy from the late Medieval period, and then by a light ship as recorded by John Rocque on his map of 1757, marking the end of The Piles. Following the decision to replace The Piles with a stone wall, work started on building the eastern terminal abutment close to the lightship in 1761 and it was quickly realised that the abutment would serve as the foundation for a lighthouse.</p> <p>John Smyth was engaged to design and build the structure in 1763. He strengthened the foundation, using caissons filled with stone and protected by a masonry buttress, and used this on which to construct a three-storey lighthouse building surrounded by an octagonal lantern of eight windows. Access was provided by an external staircase furnished with an iron balustrade that led to an iron gallery.</p> <p>The lighthouse was operational from September 1767. An apron of heavy masonry bound with cast iron was applied from 1769 and into the 1770s, and such protective works have continued to be required. In the nineteenth century, Bindon Blood Stoney deployed a series of 100-ton blocks around the base of the breakwater terminus, while the Port today has added further protection in the form of rock armour or rip-rap. The different phases of protection to the foundations are clearly visible underwater.</p> <p>In 1810 the Ballast Board assumed responsibility for all lighthouses around the Irish coast, and in 1817 Port Engineer George Halpin Senior deemed Poolbeg Lighthouse to be inadequate and required a new lantern system and additional amenities for the keepers. The changes were effected in 1819-20. The remodelled lighthouse remains substantially the same today. Resident lighthouse keepers were present until 1968, after which the site was automated and unattended.</p>			

Image/s				
Proximity to development	Outside Development area			
Likely Impacts	None			
Recommended Mitigation	None			

South side of the Channel: Other

A series of shipwreck remains are located in the sea area south of the peninsula. None of the sites will be impacted by the 3FM project and they lie some distance from the development area. They are included to complete the wider context but it is not necessary to provide more than the essential location detail on each site.

Reference	Name	Detail	ITM E	ITM N
W11566	Unknown	Recovered ship's timbers that are reburied as an archaeological storage location.	721221	733411
W11567	Unknown	Recovered ship's timbers that are reburied as an archaeological storage location.	721208	733412
W11568	Unknown	Recovered ship's timbers that are reburied as an archaeological storage location.	721197	733410
W11569	Unknown	Recovered ship's timbers that are reburied as an archaeological storage location.	721181	733409
W01734	'Ringsend Wreck'	Wooden wreck exposed during dredging operations in 2001. Oriented E-W, composite construction of timber and metal, keelson observed, carvel construction, musket balls and bullets recovered from wider area.	721314	733325
W11570	Unknown	Wooden wreck.	721314	733325
W11571	'Ringsend Wreck'	Wooden wreck.	721342	733350
W18522	Unknown	Wooden wreck measuring 16.4m long, 5.5m wide, standing 20cm proud of the seabed. Identified during a geophysical survey.	723445	733770